



Our ref: CORP F2019/000311

14 August 2019

Senator Rex Patrick
Centre Alliance
Senator for South Australia
Level 2, 31 Ebenezer Place
ADELAIDE SA 5000

BUSINESS SERVICES,
CORPORATE SERVICES
Level 16
25 Grenfell Street
Adelaide SA 5000
GPO Box 1671
Adelaide SA 5001
DX 667
Tel 8429 0422
www.pir.sa.gov.au

Dear Senator Patrick

Determination under the *Freedom of Information Act 1991*

I refer to your application made under the *Freedom of Information Act 1991* which was received by Primary Industries and Regions SA (PIRSA) on 21 June 2019, seeking access to the following:

- "1. Any documents that show the details of any grant applications by the District Council of Lower Eyre Peninsula (DCLEP) or grant approvals by PIRSA in relation to the development of a Flight Training Facility at Port Lincoln Airport.*
- 2. Any documents that show the details of any requests for funding (by loan or any other means) by DCLEP to PIRSA in relation to the development of a Flight Training Facility at Port Lincoln Airport or approvals of funding to DCLEP by PIRSA in relation to the development of a Flight Training Facility at Port Lincoln Airport."*

I apologise for the delay in responding to your application and understand that PIRSA's Freedom of Information and Privacy Officer has been in communications with your office in this regard.

Accordingly, the following determination has been finalised.

I have located twenty documents that are captured within the scope of your request.

Determination 1

I have determined that access to the following document is **granted in full**:

	Description of document	No. of Pages
1	State Commission Assessment Panel – Application on Notification	55

Determination 2

I have determined that access to the following documents is **refused**:

Doc No	Description of document	No. of Pages
2a	Attachment to Doc 2 – Draft Cabinet Submission	4
3d	Attachment to Doc 3 – Draft Cabinet Submission	5

Access to the above documents is refused pursuant to Clause 1(1)(b) of Schedule 1 of the Freedom of Information Act which states:

“1 – Cabinet documents

(1) A document is an exempt document –

(b) if it is a preliminary draft of a document referred to in paragraph (a);”

The documents consist of preliminary drafts of a proposed Cabinet Submission (whether or not it had been submitted to Cabinet).

Determination 3

I have determined that access to the following documents is **refused**:

Doc No	Description of document	No. of Pages
2	Email thread dated 4/3/2019 to 5/3/2019	3
2b	Attachment to Doc 2	2
2c	Attachment to Doc 2	2
2d	Attachment to Doc 2	2
2e	Attachment to Doc 2	1
3	Email dated 12/3/2019	1
3b	Attachment to Doc 3	2
3c	Attachment to Doc 3	3
5	Email thread dated 12/3/2019	2
6	Email thread dated 12/3/2019	2
7	Email thread dated 12/3/2019	3
9	Cabinet document undated	2
10	Email dated 8/3/2019 and attachment	3
11	Email dated 8/3/2019 and attachment	7

Access to the above documents is refused pursuant to Clause 1(1)(e) of Schedule 1 of the Freedom of Information Act which states:

“1 – Cabinet documents

(1) A document is an exempt document –

(e) if it contains matter the disclosure of which would disclose information concerning any deliberation or decision of Cabinet;”

The documents were prepared to gather information to inform Cabinet and for the preparation of a proposed Cabinet Submission and, accordingly, disclosure of these documents would reveal information concerning the proposed deliberations of Cabinet.

Determination 4

I have determined that access to the following documents is **refused**:

Doc No.	Description of document	No. of Pages
3a	Attachment to Doc 3 – Minute dated March 2019	8
4	Email dated 12/3/2019 and attachment	15
8	Email thread dated 19/3/2019 to 25/3/2019 and attachment	3

Access to the above documents is refused pursuant to Clause 7(1)(c) of Schedule 1 of the Freedom of Information Act which states:

“7 – Documents affecting business affairs

(1) A document is an exempt document –

(c) if it contains matter –

(i) consisting of information (other than trade secrets or information referred to in paragraph (b)) concerning the business, professional, commercial or financial affairs of any agency or any other person; and

(ii) the disclosure of which –

(A) could reasonably be expected to have an adverse effect on those affairs or to prejudice the future supply of such information to the Government or to an agency; and

(B) would, on balance, be contrary to the public interest”

In addressing the public interest test requirement for this exemption, I have balanced the following factors:

In favour of the public interest:

- Meeting the objects of the Act favouring access to documents.
- The importance of transparency and openness and the interest that the public has in the decision-making processes of Government.
- Ensuring optimal use of public resources.
- High level of interest in regional development.

Contrary to the public interest:

- Disclosure of these documents would reveal the business, financial, commercial and investment information and analysis detail relating to a potential project for consideration by Government. Such information is considered commercially sensitive to the organisations concerned.

- The release of the ideas, methods and sources cited throughout the documents would provide an unfair advantage to competitors in their future business proposals and pursuits.
- If third parties cannot be assured of confidentiality in relation to their commercially sensitive information, the release of this information would have the potential to harm business relationships with PIRSA and hamper future dealings with the agency for the betterment of South Australia.
- Information obtained in the course of assessment processes is required to be kept confidential for probity reasons.
- Disclosure of these documents would be expected to prejudice the future supply of information to the agency, as the level of trust in the Government of handling such information would be substantially diminished.

I have determined that disclosure of these documents would, on balance, be contrary to the public interest.

If you are dissatisfied with this determination, you are entitled to exercise your right of review and appeal as outlined in the attached documentation, by completing the "Application for Review of Determination" and returning the completed form to:

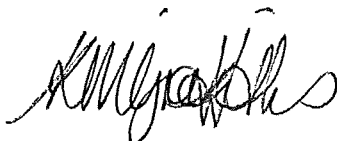
Freedom of Information Principal Officer
Primary Industries and Regions SA
GPO Box 1671
ADELAIDE SA 5001

In accordance with the requirements of Premier and Cabinet Circular PC045, details of your application, and the documents to which you are given access, will be published in PIRSA's disclosure log. A copy of PC045 can be found at http://dpc.sa.gov.au/data/assets/pdf_file/0019/20818/PC045-Disclosure-Log-Policy.pdf

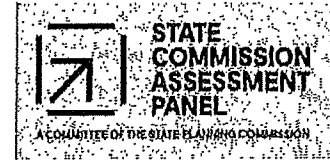
If you disagree with publication, please advise the undersigned in writing within fourteen calendar days from the date of this determination.

Should you require further information or clarification with respect to this matter, please contact Ms Lisa Farley, Freedom of Information and Privacy Officer on 8429 0422 or email PIRSA.FOI@sa.gov.au.

Yours sincerely



Michelle Griffiths
Accredited Freedom of Information Officer
PRIMARY INDUSTRIES AND REGIONS SA



APPLICATION ON NOTIFICATION – CATEGORY 3

Applicant:	Flight Training Adelaide Pty Ltd C/- Masterplan SA Pty Ltd
Development Number:	932/E003/19
Nature of Development:	Construction of an educational establishment (flight training facility) with associated accommodation, recreational, learning, aircraft hangar, office, storage, maintenance and operational facilities
Type of Development:	Merit
Zone / Policy Area:	Airfield Zone
Subject Land:	29 Barry Firth Drive, North Shields SA 5607
Contact Officer:	Benjamin Williams
Phone Number:	08 7109 7023
Start Date:	7 March 2019
Close Date:	22 March 2019
<p>During the notification period, hard copies of the application documentation can be viewed at the Department of Planning, Transport and Infrastructure, Level 5, 50 Flinders St, Adelaide, during normal business hours. Application documentation may also be viewed during normal business hours at the local Council office (if identified on the public notice).</p>	

Written representations must be received by 22 March 2019 and can either be posted, faxed, hand-delivered or emailed to the State Commission Assessment Panel.

Any representations received after the close date will not be considered.

Postal Address:

The Secretary
State Commission Assessment Panel
GPO Box 1815
ADELAIDE SA 5001

Street Address:

Development Division
Department of Planning, Transport and Infrastructure
Level 5, 50 Flinders St
ADELAIDE SA 5000

Email Address: scapreps@sa.gov.au

Fax Number: (08) 8303 0753



Government of South Australia

Department of Planning,
Transport and Infrastructure

DEVELOPMENT ACT 1993

CATEGORY 3

NOTICE OF APPLICATION FOR CONSENT TO DEVELOPMENT

Notice is hereby given that an application has been made by **Flight Training Adelaide Pty Ltd C/- Masterplan SA Pty Ltd** for consent to construct an educational establishment (flight training facility) with associated accommodation, recreational, learning, aircraft hangar, office, storage, maintenance and operational facilities (Development Number: 932/E003/19).

The land is situated at the Port Lincoln Airport, 29 Barry Firth Drive, North Shields SA 5607, being Certificate of Title Volume 6111 Folio 59.

The subject land is located within the Airfield Zone of the Lower Eyre Peninsula Council Development Plan (Consolidated on 12 July 2018).

The application may be examined during normal office hours at the office of the State Commission Assessment Panel (SCAP), Level 5, 50 Flinders Street Adelaide, at the offices of the Lower Eyre Peninsula Council and the City of Port Lincoln. Application documentation may also be viewed on the SCAP website http://www.saplanningcommission.sa.gov.au/scap/public_notices.

Any person or body who desires to do so may make representations concerning the application by notice in writing delivered to the Secretary, State Commission Assessment Panel, GPO Box 1815, Adelaide SA 5001 **NOT LATER THAN 5:00pm 22 March 2019**. Submissions may also be emailed to: scapreps@sa.gov.au

Each person or body making a representation should state the reason for the representation and whether that person or body wishes to be given the opportunity to appear before the SCAP to further explain the representation.

Submissions may be made available for public inspection.

Should you wish to discuss the application and the public notification procedure please contact Benjamin Williams on 08 7109 7023 or Ben.Williams2@sa.gov.au

Alison Gill
SECRETARY
STATE COMMISSION ASSESSMENT PANEL

PN3632

www.sa.gov.au

South Australian
DEVELOPMENT ACT, 1993
REPRESENTATION ON APPLICATION – CATEGORY 3

Applicant: Flight Training Adelaide Pty Ltd C/- Masterplan SA Pty Ltd
Development Number: 932/E003/19
Nature of Development: Construction of an educational establishment (flight training facility) with associated accommodation, recreational, learning, aircraft hangar, office, storage, maintenance and operational facilities
Development Type: Merit
Zone / Policy Area: Airfield Zone
Subject Land: 29 Barry Firth Drive, North Shields SA 5607 (Port Lincoln Airport)
Contact Officer: Benjamin Williams
Phone Number: 08 7109 7023
Close Date: 5:00 PM 22 March 2019

My Name: _____ **My phone number:** _____

Primary method(s) of contact: **Email:** _____
Postal Address: _____ **Postcode:** _____

You may be contacted via your nominated PRIMARY METHOD(s) OF CONTACT if you indicate below that you wish to be heard by the State Commission Assessment Panel in support of your submission.

My interests are:
(please tick one) ☐ owner of local property
☐ occupier of local property
☐ a representative of a company/other organisation affected by the proposal
☐ a private citizen

The address of the property affected is: _____

Postcode _____

My interests are:
(please tick one) ☐ I support the development
☐ I support the development with some concerns
☐ I oppose the development

The specific aspects of the application to which I make comment on are: _____

I: ☐ wish to be heard in support of my submission
(please tick one) ☐ do not wish to be heard in support of my submission
(Please tick one)

By: ☐ appearing personally
(please tick one) ☐ being represented by the following person
(Please tick one)

Signature: _____ **Date:** _____

Return Address: The Secretary, State Commission Assessment Panel, GPO Box 1815, Adelaide, SA 5001 /or
Email: scapreps@sa.gov.au

DEVELOPMENT APPLICATION FORM

COUNCIL: DISTRICT COUNCIL OF LOWER EYRE PENINSULA

APPLICANT: FLIGHT TRAINING ADELAIDE PTY LTD

Postal Address: C/- MASTERPLAN SA PTY LTD
33 CARRINGTON STREET ADELAIDE SA 5000

OWNER: DISTRICT COUNCIL OF LOWER EYRE PENINSULA

Postal Address: PO BOX 41
CUMMINS SA 5631

BUILDER: TO BE ADVISED

Postal Address: _____

Licence No: _____

CONTACT PERSON FOR FURTHER INFORMATION:

Name: GRAHAM BURNS - MASTERPLAN SA PTY LTD

Telephone: 8193 5600

Email: GRAHAMB@MASTERPLAN.COM.AU

Mobile: 0413 832 602

EXISTING USE:

REGIONAL AIRPORT

FOR OFFICE USE

Development No: _____

Previous Development No: _____

Assessment No: _____

☐ **Complying**

☐ **Non-complying**

☐ **Notification Cat 2**

☐ **Notification Cat 3**

☐ **Referrals/Concurrence**

☐ **DA Commission**

Application forwarded to DA

Commission/Council on:

/ /

Decision:

Type:

Date:

/ /

	Decision	Fees	Receipt No	Date
Planning:				
Building:				
Land Division:				
Additional:				
Dev Approval:				

DESCRIPTION OF PROPOSED DEVELOPMENT: EDUCATIONAL ESTABLISHMENT (FLIGHT TRAINING FACILITY)

LOCATION OF PROPOSED DEVELOPMENT:

House No: LOT 9, 10 PCS, 21, 22, 23 Street: LINCOLN HIGHWAY Town/Suburb: NORTH SHIELDS

Section No (full/part): _____ Hundred: _____ Volume: 5708 Folio: 74

Section No (full/part): _____ Hundred: _____ Volume: 5708 Folio: 75

Section No (full/part): _____ Hundred: _____ Volume: 6111 Folio: 59

LAND DIVISION:

Site Area (m²): _____ Reserve Area (m²): _____ No of Existing Allotments: _____

Number of Additional Allotments - (Excluding Road and Reserve): _____ Lease: YES: ☐ NO: ☐

BUILDING RULES CLASSIFICATION SOUGHT:

If Class 5, 6, 7, 8 or 9 classification is sought, state the proposed number of employees: Female: _____ Male: _____

If Class 9a classification is sought, state the number of persons for whom accommodation is required: _____

If Class 9b classification is sought, state the proposed number of occupants of the various spaces at the premises: _____

DOES EITHER SCHEDULE 21 OR 22 OF THE DEVELOPMENT REGULATIONS 2008 APPLY?

YES: ☐

NO: ☒

HAS THE CONSTRUCTION INDUSTRY TRAINING FUND ACT 1993 LEVY BEEN PAID?

YES: ☐

NO: ☒

DEVELOPMENT COST (Do not include any fit-out costs): \$16.35 MILLION

DEVELOPMENT REGULATIONS 2008

Form of Declaration
(Schedule 5, Clause 2A)

To: STATE COMMISSION ASSESSMENT PANEL
From: FLIGHT TRAINING ADELAIDE PTY LTD
Date of Application: 14 January 2019

Location of Proposed Development:

House Number:	-	Lot Number:	9 and 10, and Pieces 21, 22, 23
Street:	Lincoln Highway	Town/Suburb:	North Shields
Section No (full/part):	-	Hundred:	Louth
Volume:	6111, 5708, 5708	Folio:	59, 75, 74

Nature of Proposed Development:

Educational establishment (flight training facility) with associated accommodation, recreational, learning, aircraft hangar, office, storage, maintenance and operational facilities

MasterPlan SA Pty Ltd, acting on behalf of Flight Training Adelaide Pty Ltd for the development described above, declare that the proposed development will involve the construction of a building which would, if constructed in accordance with the plans submitted, not be contrary to the regulations prescribed for the purposes of Section 86 of the *Electricity Act 1996*. We make this declaration under Clause 2A(1) of Schedule 5 of the *Development Regulations 2008*.

14 January 2019

Date

Signed

Note 1

This declaration is only relevant to those development applications seeking authorisation for a form of development that involves the construction of a building (there is a definition of 'building' contained in Section 4(1) of the *Development Act 1993*), other than where the development is limited to:

- an internal alteration of a building; or
- an alteration to the walls of a building but not so as to alter the shape of the building.

Note 2

The requirements of Section 86 of the *Electricity Act 1996* do not apply in relation to:

- a fence that is less than 2.0 m in height; or
- a service line installed specifically to supply electricity to the building or structure by the operator of the transmission or distribution network from which the electricity is being supplied.

Note 3

Section 86 of the *Electricity Act 1996* refers to the erection of buildings in proximity to powerlines. The regulations under this Act prescribe minimum safe clearance distances that must be complied with.

Note 4

The majority of applications will not have any powerline issues, as normal residential setbacks often cause the building to comply with the prescribed powerline clearance distances. Buildings/renovations located far away from powerlines, for example towards the back of properties, will usually comply.

Particular care needs to be taken where high voltage powerlines exist; where the development:

- is on a major road;
- commercial/industrial in nature; or
- built to the property boundary.

Note 5

Information brochures 'Powerline Clearance Guide' and 'Building Safely Near Powerlines' have been prepared by the Technical Regulator to assist applicants and other interested persons. Copies of these brochures are available from Council and the Office of the Technical Regulator. The brochures and other relevant information can also be found at www.technicalregulator.sa.gov.au

Note 6

In cases where applicants have obtained a written approval from the Technical Regulator to build the development specified above in its current form within the prescribed clearance distances, the applicant is able to sign the form.

14 January, 2019

Development Applications Lodgement
State Commission Assessment Panel
PO Box 1815
ADELAIDE SA 5001

Dear Sir/Madam

**Re: Flight Training Adelaide –
Port Lincoln Airport**

We act for Flight Training Adelaide Pty Ltd. Our client operates the highly regarded flight training school at Parafield Airport.

With global demand for pilots continuing to grow, our client proposes to establish a flight training facility at Port Lincoln Airport. Flight Training Adelaide will continue to operate from and be headquartered at Parafield Airport.

Port Lincoln Airport is owned and operated by the District Council of Lower Eyre Peninsula. The Council has identified vacant land at the corner of the main entrance road (Barry Firth Drive) and Lincoln Highway for the development of the flight training facility. The land is identified in both the Port Lincoln Airport Master Plan, and Council's Development Plan, for commercial development.

The nature and scale of the proposal and its estimated development cost is such that the Coordinator General has agreed that the proposal should be assessed by the State Commission Assessment Panel in accordance with Clause 20 of the Development Panel Regulations 2008. We attach the Coordinator-General's letter of 14 January 2019.

Please find enclosed:

- a set of drawings of the proposed development;
- our Planning Report which contains a detailed description of the proposal and our assessment of it against the relevant provisions of the Development Plan;
- the relevant Certificates of Title; and
- a Development Application form and Electricity Declaration form.



Please issue a Tax Invoice for the associated development application fees and we will arrange for immediate payment to be made.

Yours sincerely

Graham Burns
MasterPlan SA Pty Ltd

enc: Drawing set
 Planning Report
 Certificates of Title
 Development Application form
 Electricity Declaration form

PLANNING REPORT

FLIGHT TRAINING FACILITY

AT: PORT LINCOLN AIRPORT, LINCOLN HIGHWAY NORTH SHIELDS

FOR: FLIGHT TRAINING ADELAIDE PTY LTD

1.0 INTRODUCTION

This Planning Report is prepared for the accompanying development application by Flight Training Adelaide Pty Ltd ("FTA") to develop a flight training facility at Port Lincoln Airport.

The proposal is shown on the accompanying set of drawings prepared by Atlas Living and MasterPlan SA Pty Ltd. An environmental noise assessment of the proposal has been prepared by Sonus Pty Ltd and is attached.

2.0 FLIGHT TRAINING ADELAIDE

FTA delivers world-class, customised aviation training services for the fixed wing and rotary wing aircraft industry. The company is based at Parafield Airport, South Australia. The business commenced operations in 1982 and in 2005 it was purchased by Young Brothers Aviation, based in Hong Kong.

FTA is focussed on providing future airline and helicopter captains. For this purpose, the company supplies appropriate training aircraft and flight simulators.

The business attracts sponsored cadets and self-funded students from throughout Australia and overseas. It offers a full range of services including pilot selection, *ab initio* and advanced training courses for airline and general aviation students.

Global demand for pilots is growing, with estimates indicating a need for at least an additional 600,000 pilots over the next 20 years. Boeing has projected that aviation will require 790,000 new pilots by 2037 to meet predicted demand.¹

FTA's Parafield training facility is served by two parallel runways for circuit training and includes a residential campus within the airport grounds.

FTA will continue to train and have its head office based at Parafield Airport.

¹ Pilot and Technician Outlook, Boeing Communications: July 2018



3.0 PORT LINCOLN AIRPORT

Since 1990, Port Lincoln Airport has been owned and operated by the District Council of Lower Eyre Peninsula. It is South Australia's largest and busiest regional airport, providing public landing strips and associated taxiway, apron, terminal, aircraft hangers and refuelling facilities for air transport operators. In 2013 the airport completed construction of a modern new terminal with supporting infrastructure including an RPT apron extension, new taxiway, road network and 180 space car park.

Regular passenger aircraft provide up to 12 services per day between Port Lincoln and Adelaide.

The Airport has a 3-runway layout with the capacity to handle BAC 146, Saab SF 340 and Embraer 120 airliners. The main runway 01/19 runs generally north-south. It is 1500 metres long and 30 metres wide. The airport's sealed apron can handle 3 RPT Saab-sized aircraft simultaneously. Gravel Runway 05/23 which is aligned generally south-west to north-east is to be decommissioned.

In 2013 the construction of Port Lincoln Airport's new passenger terminal was completed. The terminal building has a floor area of 2,300 square metres and has been designed to cater for a peak demand of 260 patrons passing through at any one time. Vehicle entry into and out of the airport is provided directly from Lincoln Highway. The entrance leads to a loop road which services the passenger terminal and short term public car park. Additional roads service the hangars, workshops, long term car park and other commercial facilities.

The 2016 Port Lincoln Master Plan² forecasts an annual increase of 1.5 percent in regional passenger movement (low forecast) and 3.57 percent (high forecast). All regional services are direct to and from Adelaide. Regional aircraft movement is forecast to increase by 0.5 percent (low forecast) and 1.5 percent per annum (high forecast).

The high forecast figure equates to around 9,600 aircraft movements per year or 14 flights per day, which is close to 400,000 movements per annum or 550 passenger arrivals per day. These assumptions are based on the use of 50 seat aircraft operating at 60 percent occupancy (10 flights per day), with 75 seat aircraft generating 12 flights per day.

Port Lincoln Airport has experienced steady growth in general aviation movements. General Aviation activity is made up of non-scheduled flights and includes charter, flying training, private/business, agricultural and aerial work. The Airport Master Plan high forecast is for 10,380 general aircraft movements by 2035. For planning purposes, it is assumed that these numbers will consist of an even mix of single and twin engine aircraft.

² Port Lincoln Airport Master Plan 2016 (District Council of Lower Eyre Peninsula)



4.0 PROPOSED DEVELOPMENT

4.1 Overview

FTA proposes to establish a flight training facility at Port Lincoln Airport. The facility will initially commence operations as a "soft-start" made up of:

- general aviation parking for 12 aircraft;
- lease of the old terminal building to serve as lecture rooms, dispatch and operational functions for approximately six months; and
- interim short term accommodation in nearby towns and settlements.

The "soft-start" does not require development approval.

The "hard-start" will involve the development of vacant airport land at the corner of Lincoln Highway and Barry Firth Drive, as detailed on the Development Enlargement Plan prepared by MasterPlan SA Pty Ltd. It will involve in the order of:

- 28,000 square metres of site area within the airport boundary for permanent operational and accommodation facilities;
- 12,000 square metres for aircraft hangar parking and maintenance;
- 16,000 square metres for operations in two buildings;
- 27 units of accommodation comprising 22 units for student accommodation and 5 units for staff;
- associated vehicle parking;
- associated airside services and facilities; and
- an adjacent taxi-way running parallel with the main sealed runway 01/19.

The flight training facility has been designed to accommodate 120 students with provision for between 160 to 170 beds.

There will be an overall requirement for 22 single engine DA40 aircraft. These aircraft will be based at Port Lincoln Airport and will only be used for flight training purposes.

The proposal is predicted to generate:

- 25,000 flying hours per annum; and
- 65,000 aircraft movements per annum.



Employment generation is predicted to be:

- 53 full-time equivalent direct jobs; plus
- 105 education sector indirect jobs.

4.2 Services

The development will be connected to power, communications, water, stormwater and wastewater services presently connected to the site. Wastewater will be pumped via rising main to Council's wastewater lagoons at North Shields.

4.3 Development Site

The development site is shown on the Site Context plan prepared by Master Plan SA Pty Ltd. It is irregularly shaped and bordered by Lincoln Highway to the west, Barry Firth Drive airport entrance road to the north and the Airport Terminal, General Aviation Hangars and the public car park to the east.

The site is level and devoid of trees except for a small clump near Barry Firth Drive. It falls gently from the north west to the south east. An open unlined drain is constructed on the eastern side of the site adjacent to the General Aviation hangars.

An NDB (Non-Directional Beacon) occupies the northern corner of the site near Barry Firth and Phil Wiseman Avenue. The NDB will be retained and will not form part of the development site.

Gravel runway 05/23 will be decommissioned.

4.4 Hours of Operation

Training flights are planned to commence at approximately 7.00 am and finish by approximately 11.00 pm, Monday to Friday. Weekend training flights will operate from 8.00 am until 10.00 pm.

4.5 Training Circuit

The *En Route Supplement Australia* (ERSA), managed by Air Services Australia, requires Port Lincoln Airport circuits to be to the east of the airport, over Boston Bay.

FTA's DA40 aircraft will generally use 50 per cent or less than 850 metres of the runway length to take-off. From the point of take-off, the aircraft will travel 1.2 kilometres to achieve an altitude of 500 feet, at which point a turn to the east over Boston Bay is initiated in accordance with ERSA requirements. At this point, the turning aircraft will be approximately 1 kilometre abeam of either North Shields or Poonindie township. By the time an aircraft is abeam of these settlements, it will be at an altitude of at least 750 feet, pointing 180 degrees and flying away from of Poonindie and North Shields.



When coming in to land the flightpath will not be dissimilar, but with zero engine thrust.

4.6 Development Components

The development will comprise:

- 22 student accommodation buildings;
- 5 staff accommodation buildings;
- 1 aircraft and maintenance hangar;
- 2 operations buildings;
- 1 main office/administration building;
- 2 classroom buildings;
- 1 Mess/Gymnasium building; and
- Recreational facilities including 1 or two tennis courts/basketball courts and a swimming pool.

Floor Plans, Elevations and a Site Plan of the proposed development are **attached**.

4.7 Aircraft Noise

The proposal has been assessed by Sonus Pty Ltd whose report is at **Appendix A**. Sonus have measured the noise from DA40 aircraft currently in use at Parafield and proposed to be used at Port Lincoln. Sonus tested the aircraft noise by layering the results of its findings onto the noise contours contained in the 2016 Port Lincoln Airport Master Plan. The Port Lincoln Airport Master Plan contours were derived in accordance with the methodology used in Australian Standard 2021 (AS2021).³

Sonus is of the opinion that noise intrusion into the nearest residential area at North Shields will not be increased by the use of DA40 training aircraft, in comparison to the existing fleet of aircraft using the airport, or even jet aircraft which might use the airport in future.

³ AS2021:2015 "Acoustics -- Aircraft Noise Intrusion -- Building siting and construction".



5.0 NATURE OF PROPOSED DEVELOPMENT

The proposed flight training facility is an educational establishment. This term is defined in Schedule 1 of the *Development Regulations 2008* as:

Educational establishment means a secondary school, college, university or technical institute, and includes an associated pre-school, primary school or institution for the care and maintenance of children;

A technical institute is a term which is not defined in the Regulations, but is defined in the dictionary to mean a college or school that offers education focussed on research and vocational training in which students are prepared for careers that are based on practical applications and on-the-job training.⁴ The flight training facility falls within the meaning of this term.

All associated accommodation for students and staff will be ancillary to site's principal use as a flight training facility. Relevantly, the accommodation is not self-contained and so cannot be classified as a "dwelling" because no proposed building, or part of any proposed building, will be "used as a self-contained residence". It follows therefore that the student accommodation is neither a detached dwelling, group dwelling, multiple dwelling, residential flat building, row dwelling nor semi-detached dwelling.

The proposal is best described as:

Educational establishment (flight training facility) with associated accommodation, recreational, learning, aircraft hangar, office, storage, maintenance and operational facilities.

6.0 DEVELOPMENT PLAN ASSESSMENT

The relevant Development Plan for assessment purposes is the Lower Eyre Peninsula Council Development Plan, consolidated version dated 12 July 2018. This version of the Development Plan designates Port Lincoln Airport and surrounding land in the Airfield Zone (Zones Map LEP/36).

Development at Port Lincoln Airport is also subject to the provisions of the Airport Layout Concept Plan Map LEP/1. The Concept Plan indicates that the proposed development site should be used for Commercial and Lettable Area purposes. The term "Lettable Area" is not described in the Development Plan.

Airport Noise Sensitive Locations Concept Plan Map LEP/2 identifies areas within 500 metres and 250 metres of the three runway centre-line prolongations. The noise sensitive locations extend beyond the Port Lincoln Airport perimeter. Map LEP/2 is referenced elsewhere in the Development Plan in the General Section

⁴ The Concise Oxford Dictionary



(*Residential Development; Aboriginal Housing, Orderly and Sustainable Development and Building Near Airfields*) and in the Airfield Zone section.

Airport Building Heights Concept Plan Map LEP/3 is referenced in the General Section (*Building Near Airfields, and Orderly and Sustainable Development*) but not in the Airfield Zone section.

Bushfire Protection Area BPA Map LEP/14 indicates that the main airport site, including the development site, is in a General Bushfire Risk area. The low lying parts of the airport site closest to the coast are in a Medium Bushfire Risk area.

6.1 The Airfield Zone

Zone Objective 1 advises that the zone should be developed primarily for "*aircraft operations, passenger terminals, airport and aviation-related light industrial, service industrial, warehouse and storage purposes*", while Zone PDC 1 specifies that envisaged uses will include "*aircraft related facilities*".

The proposal in its broadest sense is for aviation (flight) training purposes. As such, it is an aircraft related facility, as that term has been used in Zone PDC 1.

The site which has been set aside for aviation training purposes consists of level, vacant land at the corner of Lincoln Highway and Barry Firth Drive. This part of the airport is shown on the Airport Layout Concept Plan Map LEP/1 as "Commercial" and "Lettable Area".

The Concept Plan is silent as to the specific kinds of development which should be contained within the Lettable Area, but it is reasonable to expect that aircraft related facilities would occupy this part of the Airport site.

As the facility will be used for flight training purposes, the use of this part of the airport site for aircraft related facilities, on land leased to FTA by the District Council of Lower Eyre Peninsula, is consistent with the Airport Layout Concept Plan Map LEP/1.

Zone PDC 6 states:

- PDC 6 Commercial and light industrial development located on the airport site should:**
- (a) facilitate the more efficient operation of the airport
 - (b) be sited in defined clusters and be located in close proximity to the terminal building and/or the maintenance sheds,
 - (c) not adversely affect the amenity of surrounding land uses
 - (d) be developed in accordance with the *Concept Plan Map LEP/1 - Airport Layout*
 - (e) not have direct vehicle access to the Lincoln Highway.



The proposal will facilitate the more efficient operation of the airport by the provision of air-side facilities that will improve the safety and efficiency of the airport generally, including the construction of a new parallel taxi-way adjacent to Runway 01/19.

The proposal will also help to underpin the airport's financial standing, which in turn will allow the airport owner and operator to invest in priority airport improvements which are detailed in the 2016 Airport Master Plan (PDC 6(a)).

The entire development will be restricted to a defined part of the Airport which has been ear-marked for further development in the Development Plan)Airport Layout Concept Plan at Map LEP/1) and the Airport Master Plan (PDC 6(b)).

The closest, non-airport land uses outside the boundary of the Port Lincoln Airport are to the immediate south and west of the site. These land uses consist of grazing land. There are no houses or other sensitive uses surrounding the site, the closest house being approximately 530 metres from the site's southern perimeter airport fence, as detailed on Image 1 below (PDC 6(c)).



Image 1: 530 metre radius around nearest neighbouring dwelling

The proposal will be developed generally in accordance with the Airport Layout Concept Plan Map LEP/1, for the reasons outlined above (PDC 6(d)).



No part of the proposed development will have any direct access to Lincoln Highway (PDC 6(e)).

The proposal includes accommodation for students and staff, but the accommodation is not in the form of a "dwelling" as that term is defined in Schedule 1 of the *Development Regulations 2008*.⁵

Zone Principle 7 requires dwellings to be no closer than 500 metres to the airport runways. The Principle is more relevant to dwellings outside of the Port Lincoln Airport but in any event the accommodation units are not dwellings because they will not be occupied or used as a self-contained residence. Regardless of how the accommodation is classified, no accommodation unit will be within 500 metres of the main runway 01/19.

Gravel Runway 05/23 is close to the development site, but this runway will be decommissioned.

The proposal satisfies all relevant provisions of Zone Principle 8, with the development being more than 100 metres from coastal mean high water mark, and well removed from coastal sand dunes, native vegetation and low lying areas closer to the coastline.

The development site falls within 250 metres of Runway 05/23 Shown on Concept Plan Map LEP/2 – Airport Noise Sensitive Locations, but as previously advised this runway is to be decommissioned as part of the development. Airfield Zone PDC 8 (c) does however exclude buildings, structures and "other land uses compatible with the aviation activities of the Port Lincoln Airport". The proposal is entirely compatible with the airport's aviation activities, and the airport's aviation activities will in turn complement the proposal's operational requirements.

6.2 Building Near Airfields

Those provisions of the Development Plan under *General Section: Building Near Airfields* do not apply to the proposal because the development will consist of buildings *within* the airfield, not *near* it.

Even if these provisions were relevant, the proposal satisfies the following Objective and Principles of *General Section: Building New Airfields*:

- The development has been designed to complement and enhance the airport's long-term operational and safety requirements by the addition of a high strength taxi-way runway parallel to and for the full length of main runway 01/19, and decommissioning of Gravel Runway 05/23 (Objective 1);
- All buildings will be single storey structures in accordance with Concept Plan Map LEP/23 – Airport Building Height (PDC 2);

⁵ Dwelling means a building or part of a building used as a self-contained residence [our emphasis].



- All buildings will have a finished floor level which is well above 2.75 metres AHD, this being the minimum floor level required for buildings in nearby North Shields in the Settlement Zone. The site of the proposed development has an elevation ranging from 8.9 metres AHD alongside Barry Firth Drive, down to 6.4 metres AHD next to Runway 05/23 (PDC 3);
- The development has been designed and sited in accordance with the safety requirements of the airport operator the District Council of Lower Eyre Peninsula. The criteria listed in PDC 4 are part of those requirements;
- Outdoor lighting will be restricted to bollard style, low level illumination for easy wayfinding between the car park and nearby buildings at night time (PDC 5);
- With Runway 05/23 decommissioned, the accommodation facilities will not be located within the core airport noise sensitive locations which are shown on Concept Plan Map LEP/2 – Airport Noise Sensitive Locations⁶ (PDC 7); and
- The Aircraft Noise Assessment prepared by Sonus Pty Ltd concludes that areas affected by aircraft noise will be consistent with AS 2021: Acoustics – Airport Noise Intrusion – Building Siting and Construction (PDC 8).

6.3 Interface Between Land Uses

General Section: Interface Between Land Uses PDC's 1 and 7 require development to not detrimentally affect the amenity of a locality or cause unreasonable interference from noise emissions, and for development to achieve the relevant Environmental Protection (Noise) Policy criteria. Sonus Pty Ltd were engaged to assess the impact of flight training aircraft and the amenity of areas surrounding the airport. Sonus' report is at Appendix A.

Sonus tested noise levels from DA40 aircraft currently operating at Parafield Airport. Sonus advise that the noise measurements taken at Parafield correlate closely with the Air Services Australia report entitled "Short Term Monitoring Program Parafield SA" dated 29 June 2016. Sonus then overlaid the noise contour for the Diamond DA40 aircraft onto the noise contours contained in the Port Lincoln Airport Master Plan, and note that:

⁶ Core airport noise sensitive locations are taken to be those areas within 250 metres of the runway centre-line prolongations shown on Concept Plan Map LEP/2.



Noise in the form of 70dB(A) contours into the residential area [of North Shields, the closest residential area to the airport] will not be increased by the DA40 in comparison to the existing fleet or potential future jet aircraft.

We concur with Sonus' findings.

7.0 CONCLUSIONS

We are of the opinion that the proposal by Flight Training Adelaide Pty Ltd to develop a flight training facility at Port Lincoln Airport is not seriously at variance with the Development Plan and is deserving of Development Plan Consent.

In particular the proposal:

- will attract investment and employment into the region;
- makes effective use of vacant land within the airport perimeter that has been designated for aviation related purposes;
- is consistent with the provisions of the 2016 Airport Master Plan for Port Lincoln Airport;
- will result in additional air-side services being established to improve the overall efficiency and safety of the airport;
- is a kind of development that is envisaged in the Airfield Zone;
- represents a compact and contiguous extension of Port Lincoln Airport's landside and airside buildings and services;
- will be sited on land that is well drained and not susceptible to flooding or inundation;
- is not expected to interfere with or disturb the amenity of the surrounding locality;
- will be provided with safe and convenient access to Barry Firth Drive from a dedicated driveway;
- can be connected to the full range of existing utility services; and
- will not increase noise intrusion into nearby residential areas by the use of Diamond DA40 training aircraft compared to aircraft currently using, and potential future jet aircraft that might use, the airport.



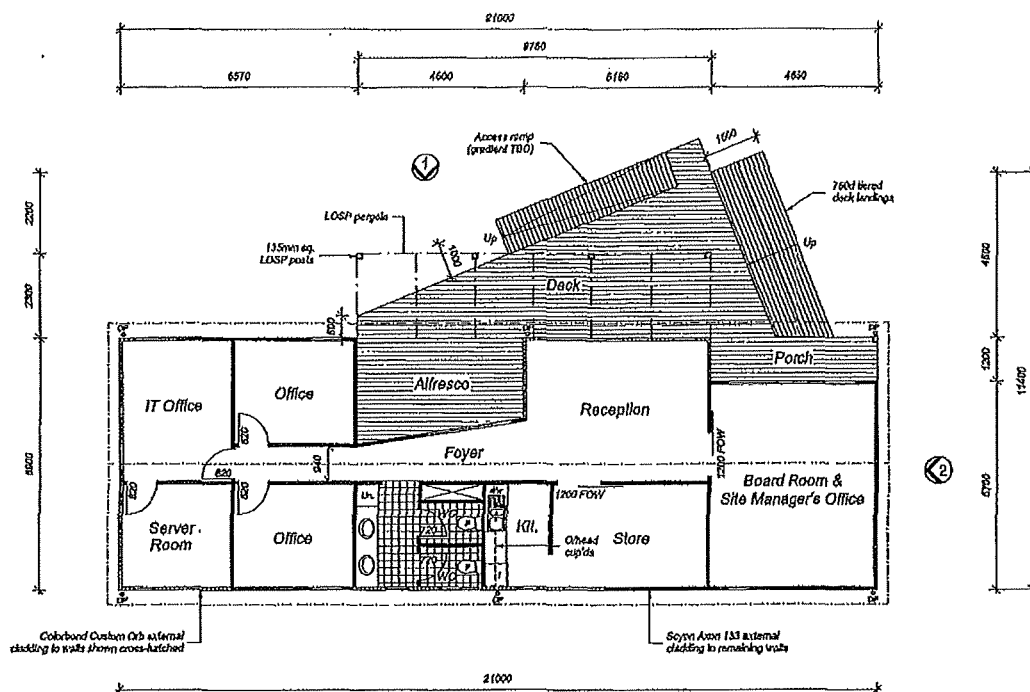
We conclude that the proposal is deserving of Development Plan Consent.

MasterPlan SA Pty Ltd

Graham Burns FPIA
B/A in Planning

14 January 2019

STAGE	DATE	DATE
PRELIM	MR	19/12/18



Floor Plan

ATLAS LIVING
 CONTEMPORARY LIVING SPACES & LINKED EXTENSIONS
 10 HONORABLE ROAD PTY LTD 150 Railway Terrace, Willebrae WA 6101
 PHONE 8411 3200 1460 (toll free) 08 9411 3200 www.atlasliving.com.au
 UIC NO 17157 ABN 61 17157 171

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AREAS	M ²
OFFICE SPACE	127.81
ALFRESCO	(11.69)
POUCH	(9.69)
DECK	47.27
TOTAL	196.46

PROPOSED Preliminary Concept Drawings

AT

FOR

FTA Project

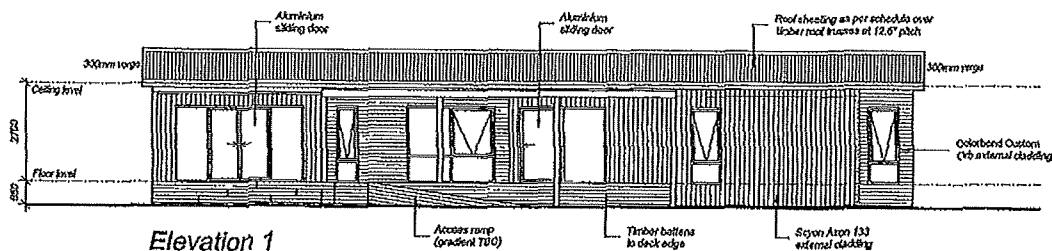
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DATE	19/12/18		
CHECKED			

STAGE	RHF	DATE
PRELIM	MR	19/12/18

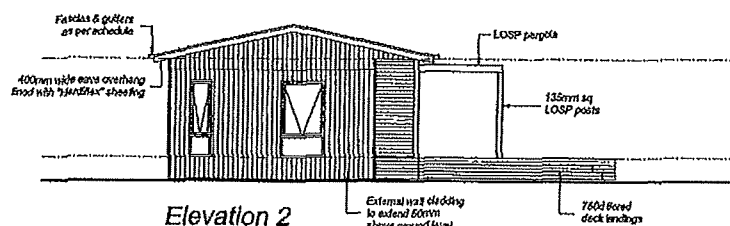
OWNER

BUILDER

DATE



Elevation 1



Elevation 2

Note: Aluminium windows as selected.
* All Door & Window glazing to be in accordance with AS1288-2006.



CONTEMPORARY LIVING SPACES & LIMITED EXTENSIONS

LOUNDRIDGE GROUP PTY LTD 138 Railway Terrace, Lake End SA 5001
P 08 8361 6380 E info@loundridge.com.au W www.atlasliving.com.au
VIC REG 173527 APT 05 (M111302)

Owner Design

DESIGN: **Owner Design**
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AREAS :	M ²
GROSS SPACE	127.81
MURRICO	(11.69)
POUCH	(8.68)
SCREEN	47.27
Final noted dimensions	
TOTAL	174.88

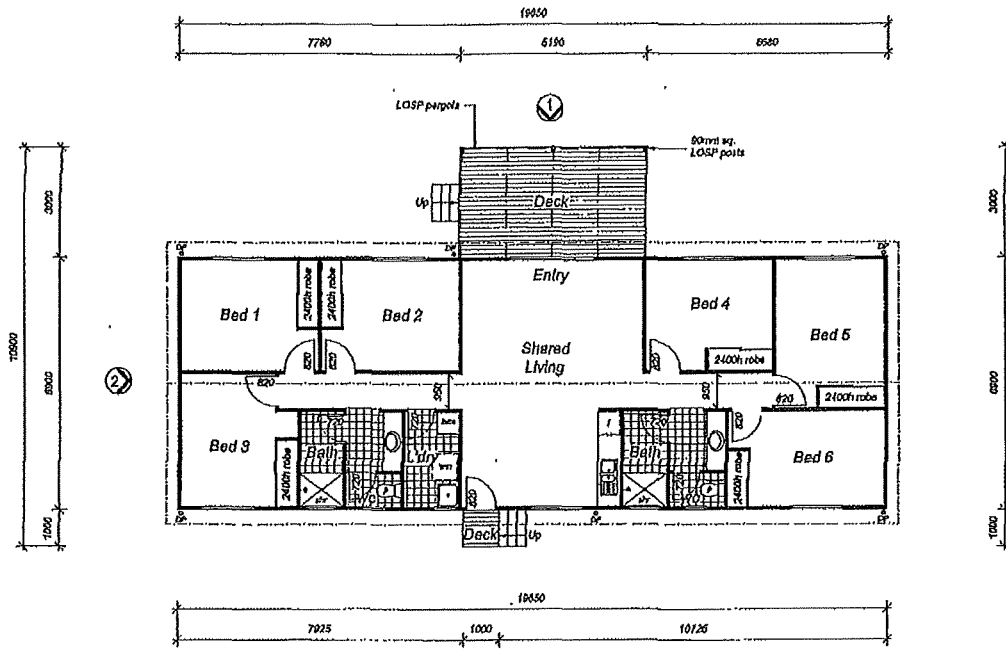
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AT

FOR: FTA Project

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DESIGN	1/1/1	SCALE 1:100
DATE	18/12/18	
DRAWN		

STAGE	PREPARED BY	DATE
PRELIM	MR	13/12/18



OWNER _____

DESIGNER _____

DATE _____

Floor Plan

ATLAS LIVING
 CONTEMPORARY LIVING SPACES & LINKED EXTENSIONS
 LUNSHOUSE GROUP PTY LTD 139 Railway Terrace, Nareed SA 5001
 P (08 831) 8383 info@atlasliving.com.au www.atlasliving.com.au
 LC 83 11211 AL 61 12/18/21

DESIGN: Owner Design
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AREAS:	M ²
ENTRY	15.58
DECK	1.00
KITCHEN	16.67
TOTAL	162.15

PROPOSED: Preliminary Concept Drawings

At:

FOR: FTA Project

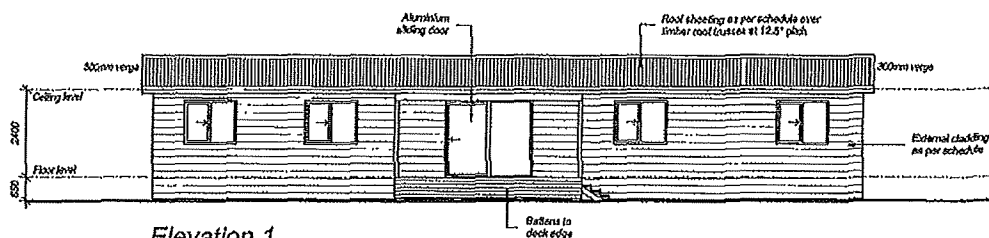
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DATE	13/12/18	
DRAWN		

STAGE:	REV:	DATE:
PRELIM	MR	13/12/18

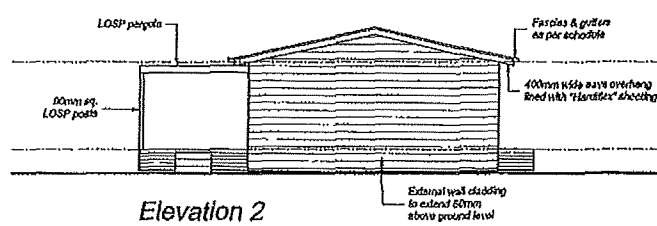
OWNER _____

ORDER _____

DATE _____



Elevation 1



Elevation 2

EXTERNAL SCHEDULE	
Roof Sheetting:	Ironstone
Parade / Gutter:	Ironstone
LOSP Pergola:	Ironstone
Window Double:	Clear Oil
Smooth Weatherboard:	Silver Grey Poshite
Aluminium Window Frame:	Silver

Note: Aluminium window as selected.
* All Door & Window glazing to be in accordance with AS 1288-2006.



CONTEMPORARY LIVING SPACES & LIVING EXTENSIONS
LIVINGSPACE GROUP PTY LTD 158 Railway Terrace, 31st Fl Sydney NSW 1585
P 02 9551 8899 E info@livespacegroup.com.au W www.livespacegroup.com.au
VIC R19 02527 454 45 303 117 877

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AREAS:	
WALL	135.68
ROOF	1.00
PERGOLA	15.67
TOTAL	152.35

PROJECT: Preliminary Concept Drawings

AS:

FOR: FTA Project

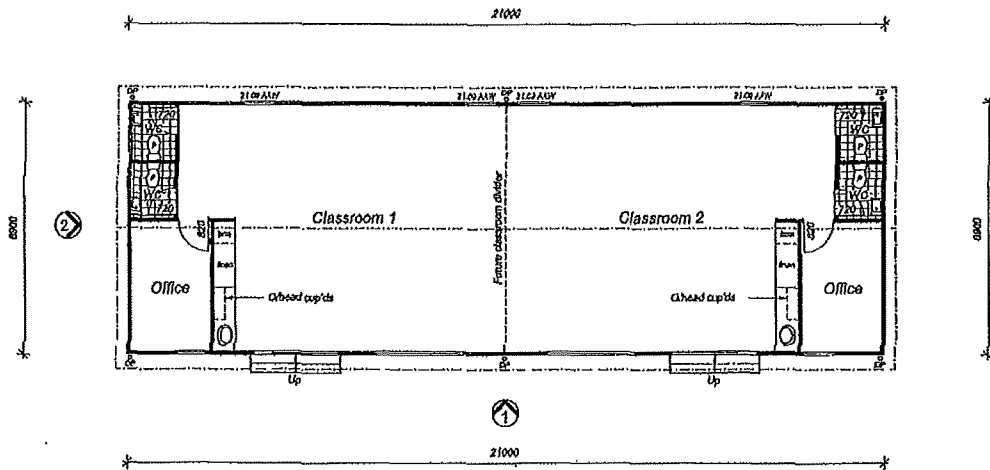
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DATE:	13/12/18
DESIGN:	

STAGE	REV.	DATE
PRELIM	NR	14/12/18

OWNER _____

BUILDER _____

DATE _____



Floor Plan

ATLAS LIVING
 CONTENT/PORARY LIVING SPACES & LUXURY EXTENSIONS
 LONGHURST GROUP PTY LTD 155 Easbury Terrace, Epping NSW 2101
 Phone 02 9318 1818 Email info@atlasliving.com.au
 Lic. No. 15550 1/24 of 10/10/17

Owner Design
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CLASSROOM FACILITY	144.50
TOTAL	144.50

PACKAGE: Preliminary Concept Drawings
 ATT: _____
 FOR: FTA Project

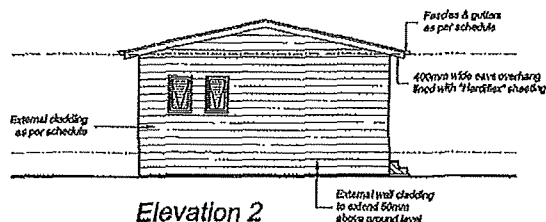
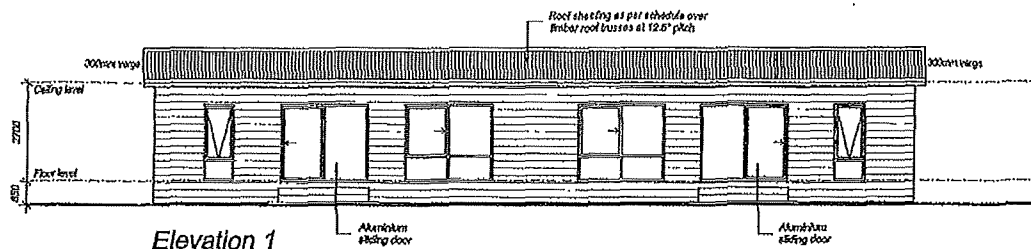
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DATE	14/12/18		
DRAWN			

STAGE:	DATE:
PRELIM	14/12/10

OWNER: [Redacted]

BUILDER: [Redacted]

DATE: [Redacted]



EXTERNAL SCHEDULE

Roof Cladding: Ironstone
Fascia / Gutter: Ironstone
LOSP Paving: Ironstone
Timber Deck: Clear Oil
Smooth Weatherboard: Silver Grey Poplar
Aluminium Window Frame: Silver

Note: Aluminium windows as selected.
* All Door & Window glazing to be in accordance with AS1288-2000.

ATLAS LIVING
CONTEMPORARY LIVING SPACES & LINKED EXTENSIONS
LONGHIDE GROUP PTY LTD, 158 Railway Terrace, W's End SA 5021
PHONE: 08 8318 1100 FAX: 08 8318 1101 WWW: atlasliving.com.au

Owner Design

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AREAS:	M ²
CLASSROOM FACILITY	144.90
TOTAL	144.90

PROJECT: Preliminary Concept Drawings

AT:

FOR: FTA Project

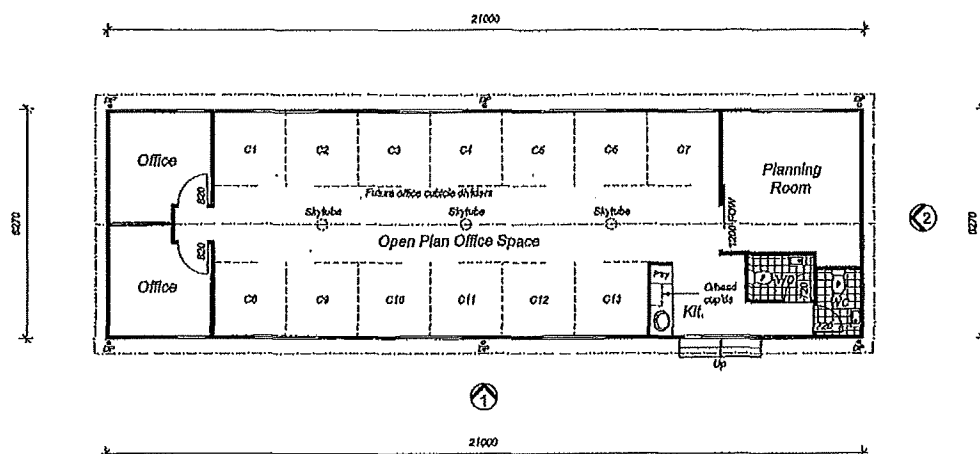
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STAGE	WHS	DATE
PRELIM	MR	17/12/18

OWNER _____

BUILDER _____

DATE _____



Floor Plan

ATLAS LIVING
 CONTEMPORARY LIVING SPACES & LIMITED EXTENSIONS
 LONKIDGE GROUP PTY LTD 108 Railway Terrace, WY16 6SA G27N
 P: 01203 552111 M: 07584 561616 E: info@atlasliving.co.uk
 (C) 2018 17/12/18 17/12/18 17/12/18

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AREAS:	M ²
OFFICE FACILITY	131.87
TOTAL	131.87

PROPOSED: Preliminary Concept Drawings

AN

FOR: FTA Project

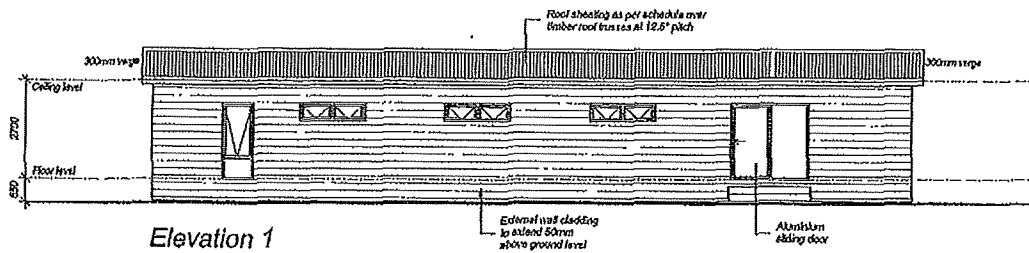
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PRELIM	17/12/18

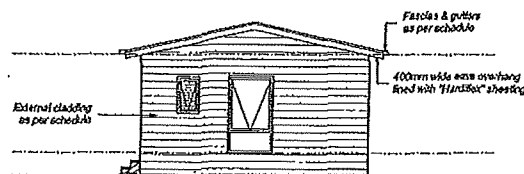
OWNER

BUILDER

DATE



Elevation 1



Elevation 2

EXTERNAL SCHEDULE

Roof Sheet: Ironstone
Fascia / Gutter: Ironstone
LOSP Purlin: Ironstone
Timber Deck: Clear Oil
Smooth Weatherboard: Silver Grey Pebble
Aluminium Window Frame: Silver

Note: Aluminium windows as subject.
* All Door & Window glazing to be in accordance with AS1288-2008.

ATLAS LIVING
CONTEMPORARY LIVING SPACES & LINKED EXTENSIONS
LONDONDE GROUP PTY LTD 155 Railway Terrace, BAH EVIDA 501
PH: 8201 8183 info@atlasliving.com.au www.atlasliving.com.au
LIC NO: 17547-001-0-100111111

Owner Design

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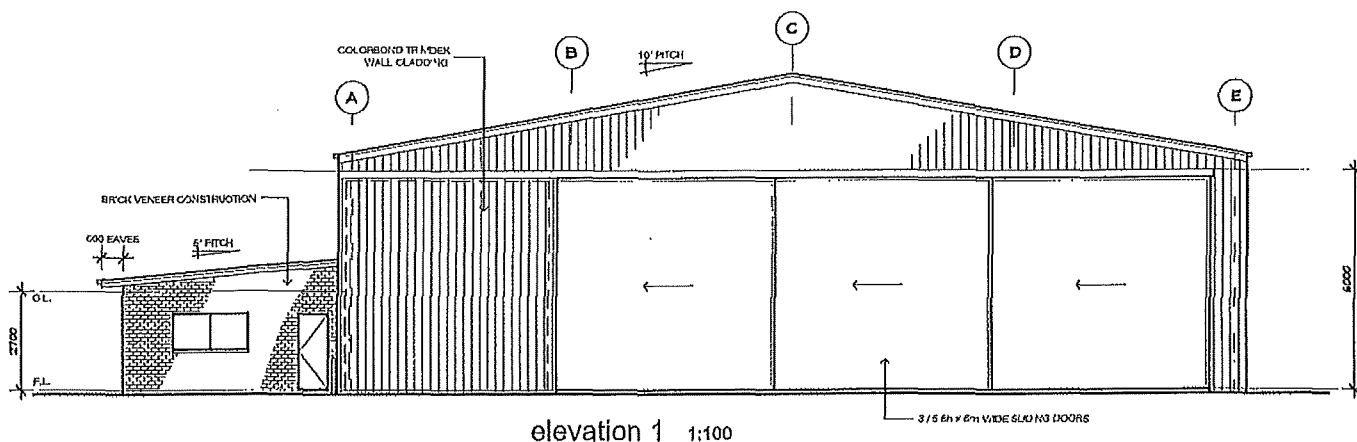
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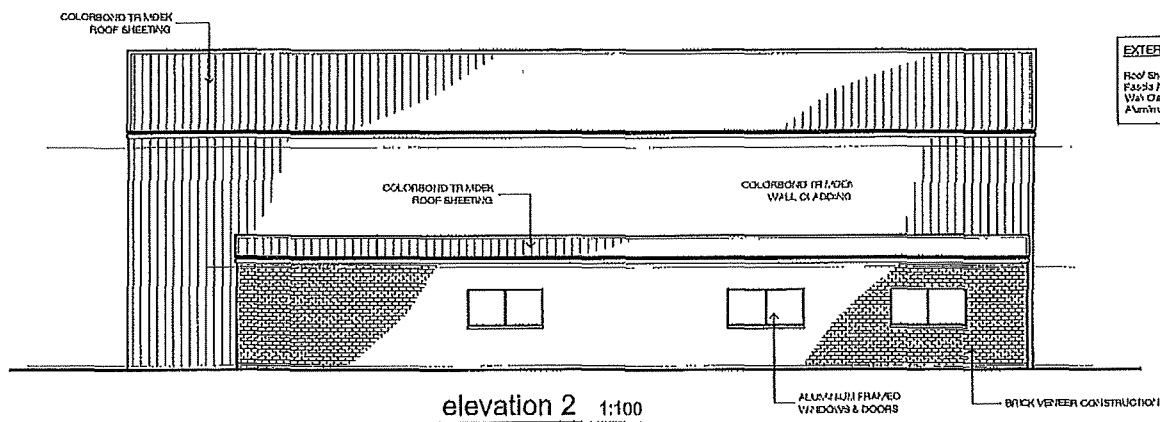
AT:

FOR: FTA Project

REV NO.	SPECIAL	2/2
DATE	17/12/18	SCALE: 1:100
DRAWN BY		
CHECKED		



EXTERNAL SCHEDULE	
Roof Sheetting:	Ironstone
Face of Form:	Ironstone
Wall Cladding:	Silver Grey Pebble
Aluminum Windows & Doors:	Silver



matt tyndale building design

email: mtyndale7@hotmail.com

Scale 1:100 @ A3

Drawn: M A T

Date: 18/12/18

Issue: A

Client: FLIGHT TRAINING ADELAIDE PTY LTD

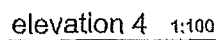
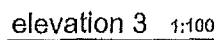
Project: PROPOSED AIRCRAFT HANGAR

Location: PORT LINCOLN AIRPORT

Sheet: 2 of 4

Dwg. No: 15091

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EXTERNAL SCHEDULE

Pool Shelving:	Yerstone
Fence / Gutter:	Ironstone
Wet Cladding	Silver Grey Pebble
Aluminium Window Frame	Silver

matt tyndale building design

Scale: 1:100 @ A3

Date: 18/12/18

Client: FLIGHT TRAINING ADELAIDE PTY LTD

Project: PROPOSED AIRCRAFT HANGAR

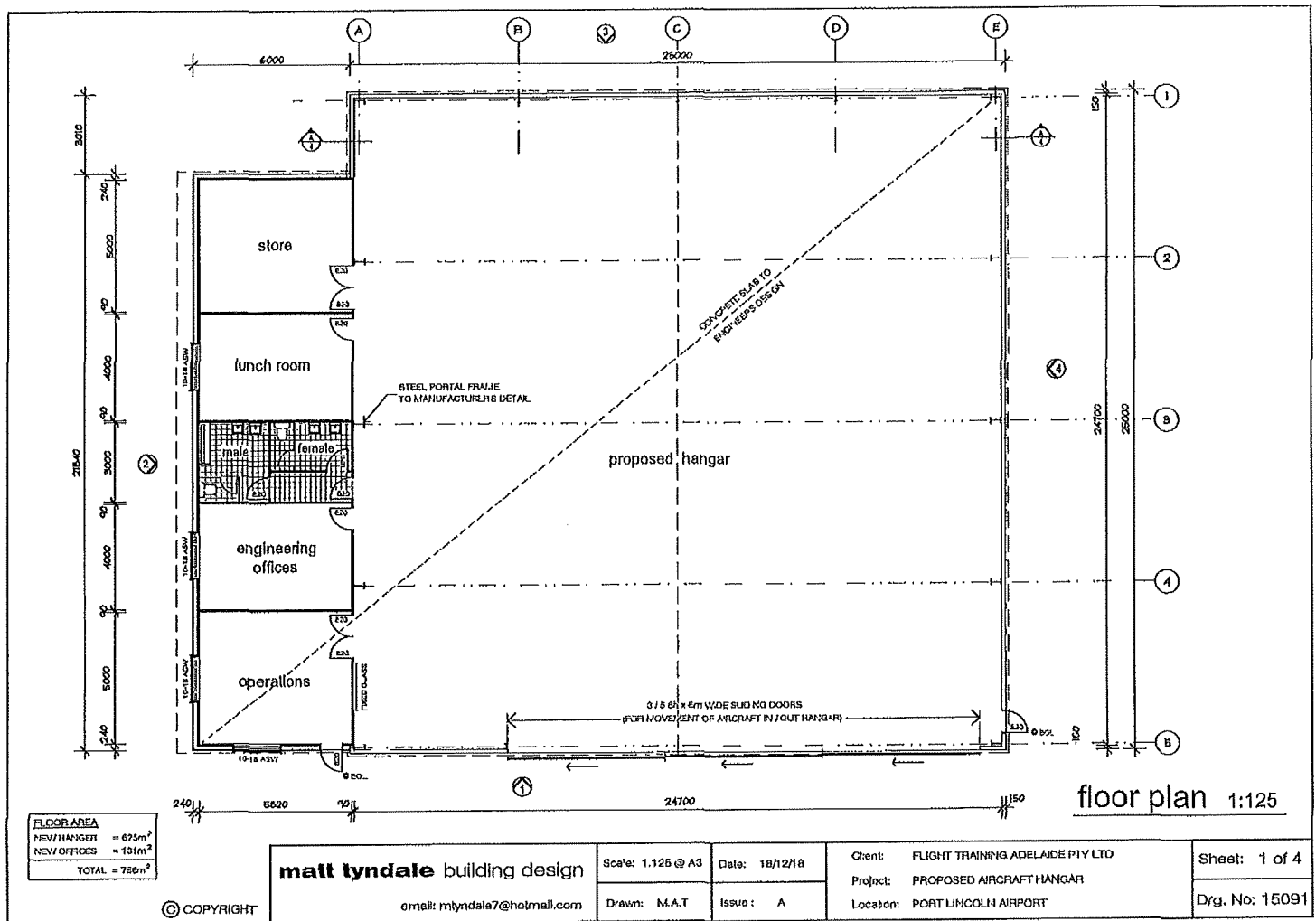
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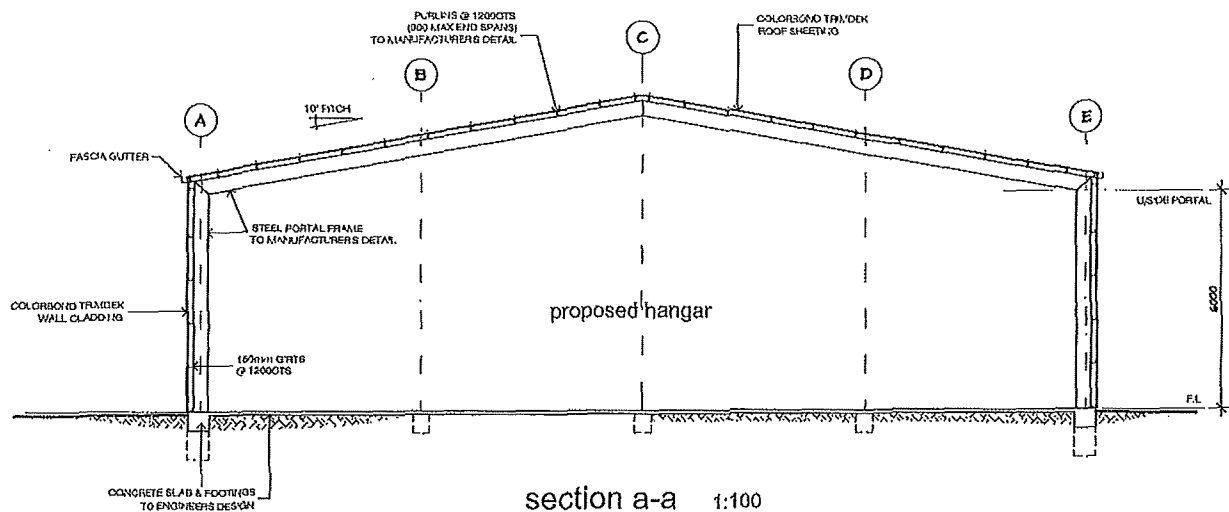
Sheet: 3 of 4

Drg. No: 15091

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email: mtyndale7@hotmail.com





section a-a 1:100

CONCRETE SLABS, FOOTINGS
STEELWORK AND WIND
BRACING TO ENGINEERS DETAIL

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email: mtyndale7@hotmail.com

Scale: 1:100 @ A3

Date: 18/12/18

Drawn: M.A.T

Issue: A

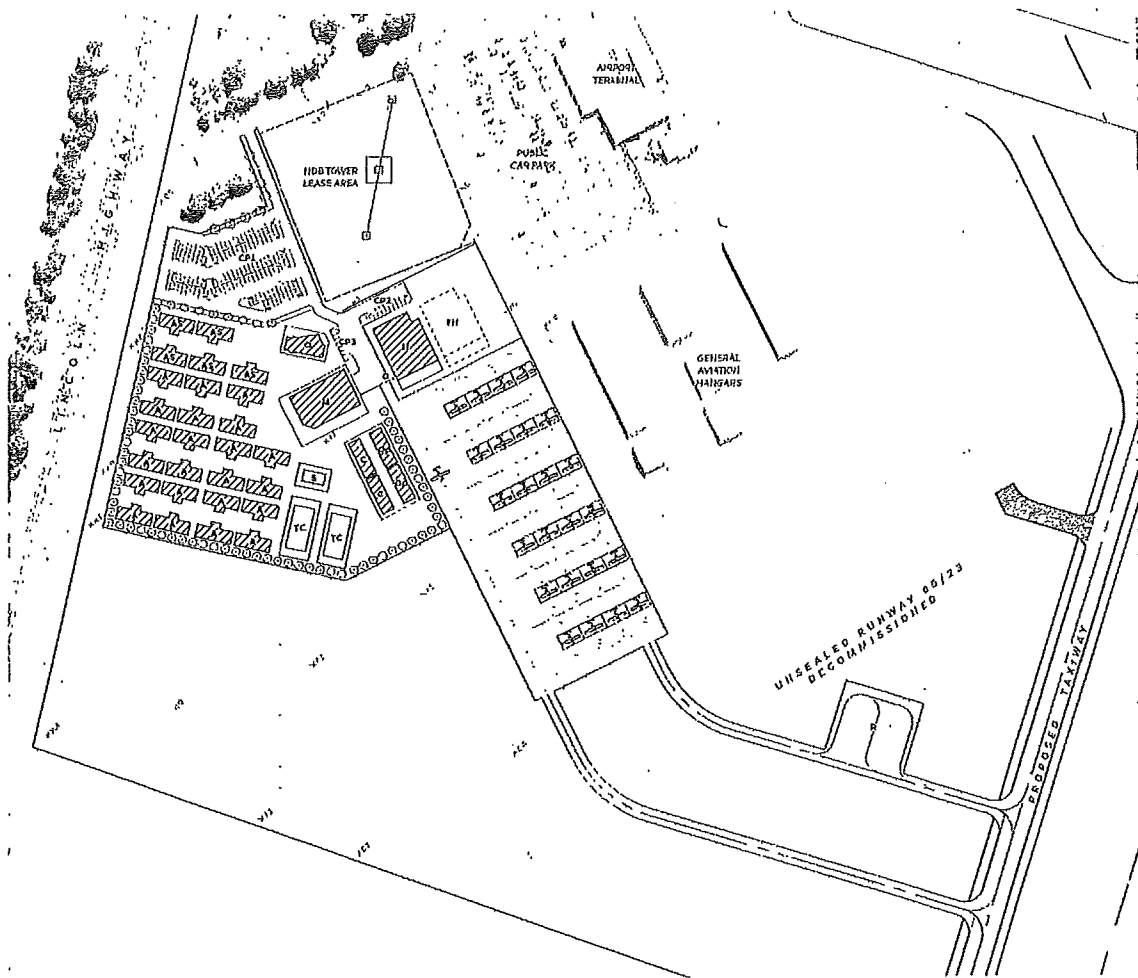
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Project: PROPOSED AIRCRAFT HANGAR

Location: PORT LINCOLN AIRPORT

Sheet: 4 of 4

Drg. No: 15091

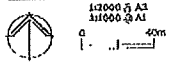


- Port Lincoln Airport
- A Accommodation Unit
 - AS Accommodation Unit Staff
 - C Classroom
 - CP1 Student / Staff Car Park (125 Spaces)
 - CP2 Operations Car Park (10 Spaces)
 - CP3 Staff Car Park (7 Spaces)
 - FH Future Hangar
 - G Security Gate
 - H Hangar
 - M Mess and Gymnasium
 - O Office
 - Op Operations
 - R Runup Bay
 - S Swimming Pool
 - TC Tennis Court / Basketball Court
 - Security Fencing
 - Proposed Landscaping
 - Spot Levels - AHD
- Note: For detail of proposed buildings please see floor plans and elevations by others.

DEVELOPMENT ENLARGEMENT

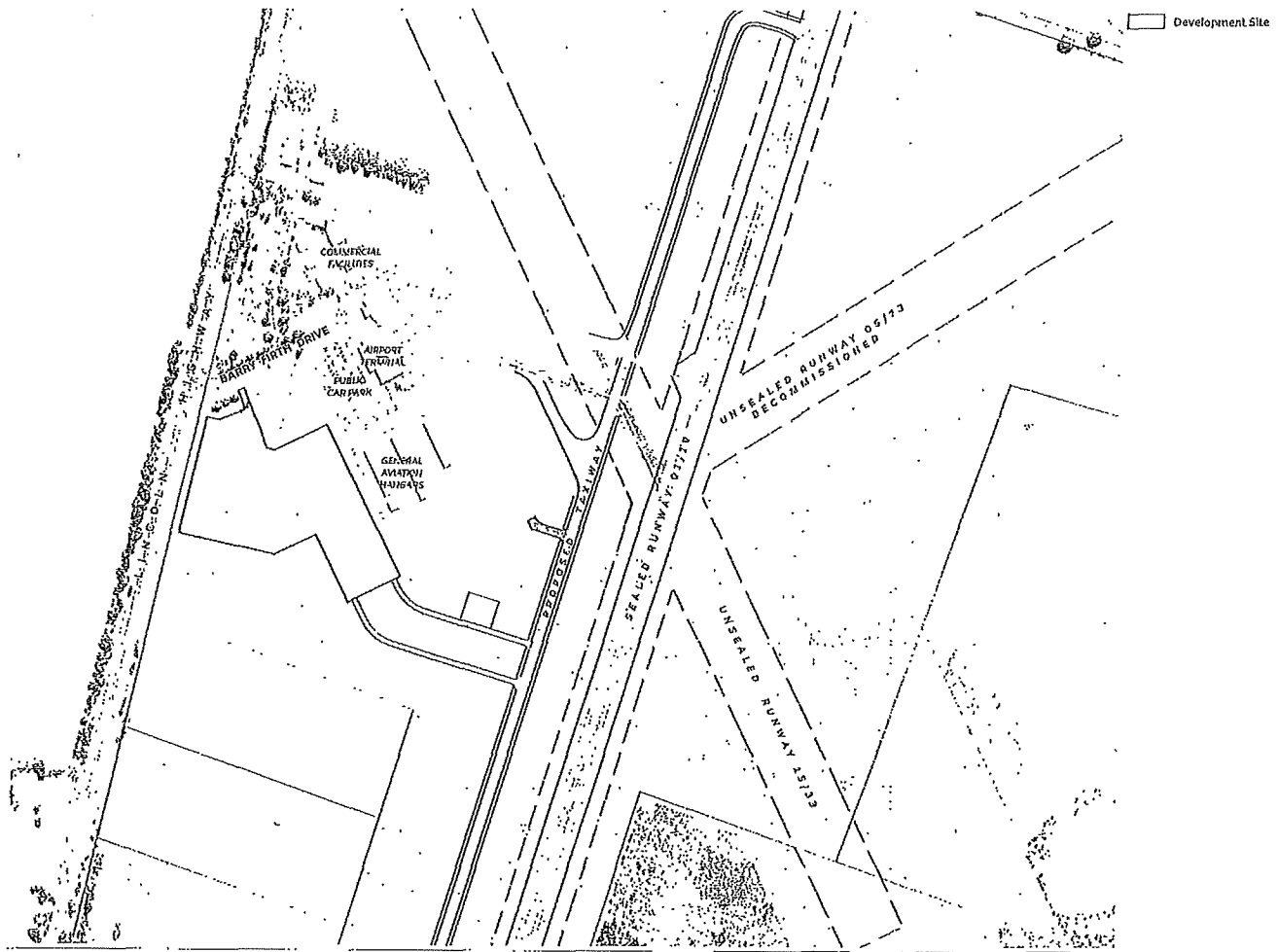
Flight Training School
PORT LINCOLN

for Flight Training Adelaide



MASTERPLAN.COM.AU
SA | NT | QLD
6 JAN 2019 DS11142-1.1





SITE CONTEXT

Flight Training School
PORT LINCOLN

for Flight Training Adelaide

MASTERPLAN.COM.AU
SA | NT | QLD
17 JAN 2019 05:31:47-L1

MASTERPLAN
SOLITE & CO. PTY LTD



- Port Lincoln Airport
- Zone Boundary
- Aq Aquaculture
- CstCon Coastal Conservation
- CstOs Coastal Open Space
- Cu Community
- PrPro Primary Production
- Se Settlement

Locality Plan PORT LINCOLN AIRPORT

PORT LINCOLN

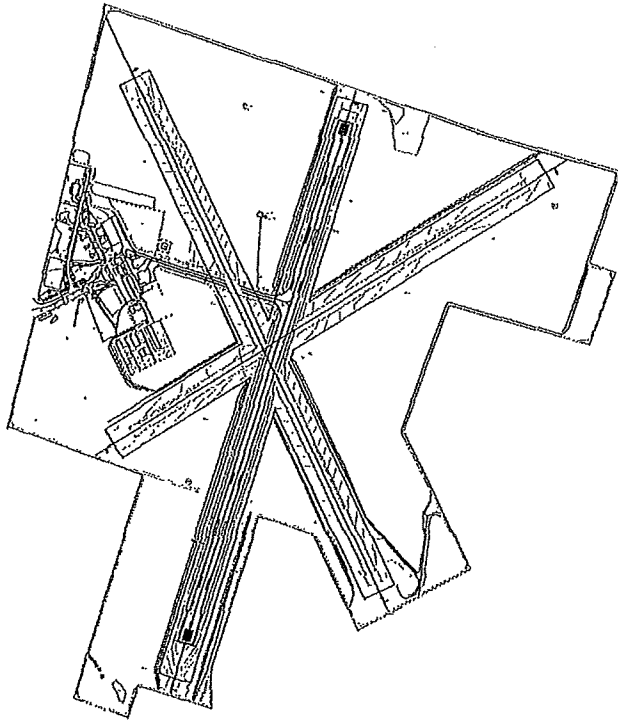
for Flight Training Adelaide Pty Ltd



1:50,000 A3
0 10km

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SA | NT | QLD
6 DEC 2010 DS:51142 1.0





Port Lincoln Airport
SURVEY AND SERVICES PLAN (2011)

Flight Training School
PORT LINCOLN

for Flight Training Adelaide



1:10000 G A3
1:10000 G A1
0 200m

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1 JAN 2019 D5-S1142-1.0





APPENDIX A

NOISE REPORT - SONUS

FTA Flight Training Facilities

Port Lincoln Airport

Aircraft Noise Assessment

S5875C2

December 2018

Chris Turnbull

Principal

Phone: +61 (0) 417 845 720

Email: ct@sonus.com.au

www.sonus.com.au

**FTA Flight Training Facilities – Port Lincoln Airport
Aircraft Noise Assessment
S5875C2
December 2018**

Document Title : FTA Flight Training Facilities Port Lincoln Airport
Aircraft Noise Assessment

Document Reference : S5611C4

Date : December 2018

Author : Chris Turnbull, MAAS

Reviewer : Jason Turner, MAAS

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INTRODUCTION

Flight Training Adelaide (FTA) is currently located at Parafield Airport, South Australia and provides customised aviation training solutions for the fixed wing and rotary wing industry. FTA is proposing to expand its training facilities to include fixed wing aircraft at Port Lincoln Airport. FTA proposes to use 16 "Diamond DA-40" aircraft for the expansion with an estimated 65,000 movements per year.

The Port Lincoln Airport is owned and operated by the District Council of Lower Eyre Peninsula and is located near the township of North Shields, ten kilometres north of the City of Port Lincoln. The land is located within the Airfield Zone within the Lower Eyre Peninsula Council Development Plan (consolidated 12 July 2018). The airport has in the vicinity of 14,500 aircraft movements and almost 170,000 passengers annually with up to 24 scheduled daily passenger flights, making it the busiest regional airport in South Australia. Current regular passenger transport operations at Port Lincoln include regional "turbo prop" Saab 340 (Regional Express) and Bombardier Dash 8 300 (Qantaslink). In the longer term it is anticipated these aircraft may be replaced or supplemented by Avions De Transport Regional ATR 42/72, Embraer ERJ 170/175/190 and Bombardier Dash 8 400 aircraft.

MASTERPLAN

The “Port Lincoln Airport Masterplan” was prepared in 2016 (the Masterplan). As part of the Masterplan, single noise level event contours were prepared. These contours indicate the area where the noise from each aircraft type will exceed 70 dB(A) based on the data for the specific aircraft in Australian Standard 2021¹ (AS2021). The Masterplan states:

Typically the 70 dB(A) contour has been used in studies undertaken by Department of Transport and Regional Services, as it is equivalent to a single event level of 60dB (A) specified in the Australian Standard 2021, as the accepted indoor design sound level for normal domestic dwellings. (An external single noise event will be attenuated by approximately 10 dB (A) by the fabric of a house with open windows) An internal noise level above 60 dB(A) is likely to interfere with conversation or listening to the television.

The noise contours from the Masterplan is shown in Appendix A. The contours show the noise for three broad groups of aircraft:

- Turbo prop - Saab 340 (current Regional Express aircraft),
- Turbo prop Dash 8 300/Q400 (current Qantaslink aircraft and potential future aircraft),
- Jets – Embraer 170/175/190 (potential future aircraft).

The Masterplan summarises the contours in the following way:

Noise data in the form of 70 dB(A) contours for each group of aircraft was plotted from AS2021 which provides noise levels at various distances and offsets from the runway ends based on actual measurements recorded at Sydney Airport. The contours show:

- *Noise intrusion by the 70 dB(A) noise level into residential area in North Shields from current Saab 340 operations will be reduced significantly when the existing fleet is replaced with later model turbo prop aircraft equivalent to Q400 aircraft.*
- *Introduction of jet aircraft will generate single event noise levels above current levels,*

This report supplements the Masterplan contours by overlaying the “Diamond DA-40” aircraft in the same format and assessment methodology as the Masterplan.

¹ AS2021:2015 “Acoustics - Aircraft noise intrusion – Building siting and construction”

PROPOSED AIRCRAFT CONTOUR

FTA proposes to use the Diamond DA40, which is a single engine fixed pitch propeller aircraft. The noise from the specific aircraft is not listed in AS2021, but data are provided for a "Generic 1-engine FP Prop". To supplement the data, the noise from the aircraft was measured at Parafield Airport and reference is made to an Air Services Australia report titled, "Short Term Monitoring Program Parafield SA" dated 29 June 2016. This report provides specific measurements of the Diamond DA-40 aircraft.

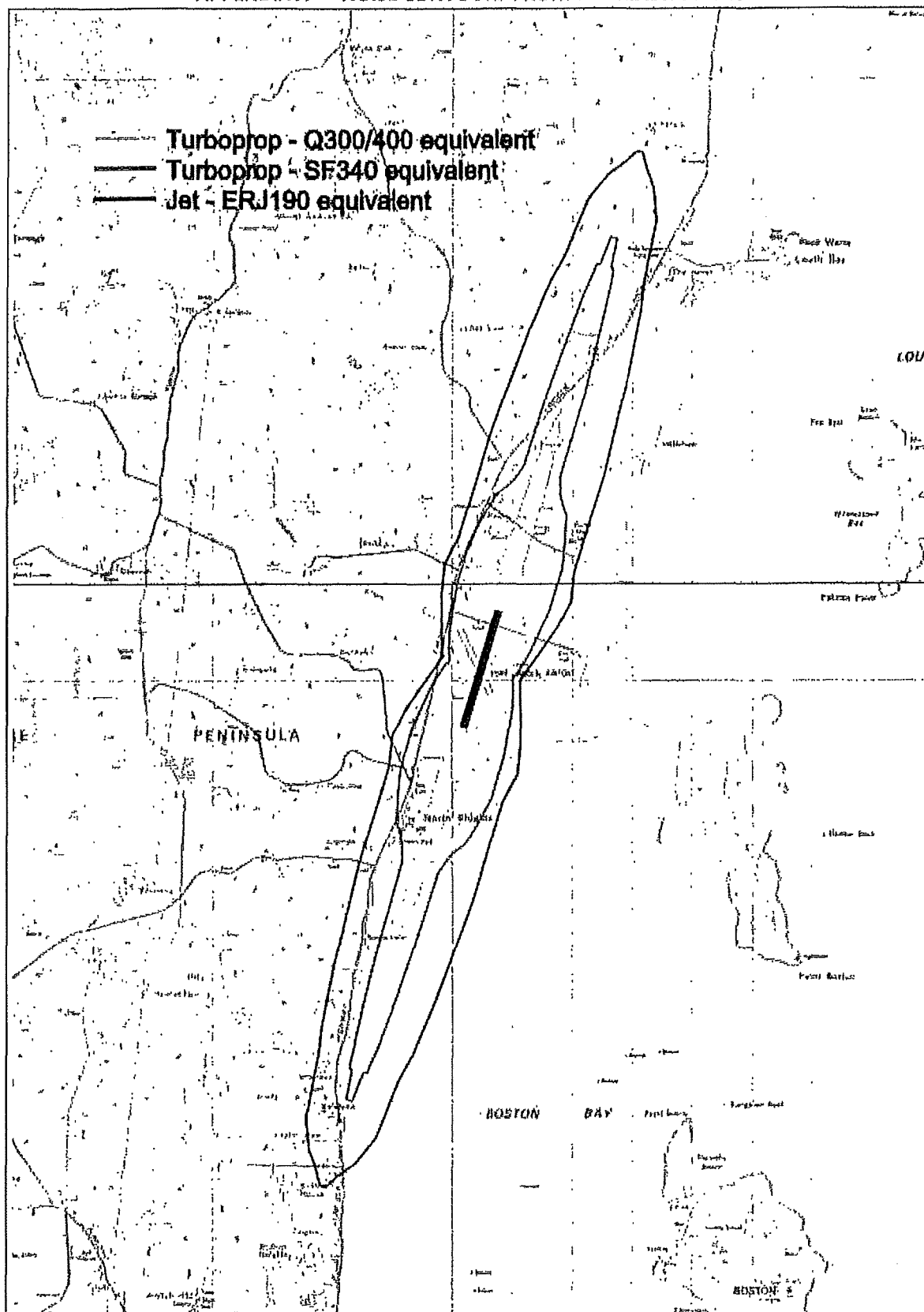
The measurements at Parafield and the Air Services Australia report showed good correlation with the generic noise data in AS2021 and therefore the data from AS2021 have been used. The noise contour for the Diamond DA40 has been overlaid on the noise contours from the Masterplan and is attached as Appendix B.

Based on the above, the Masterplan summary with respect to the contours could be updated as follows:

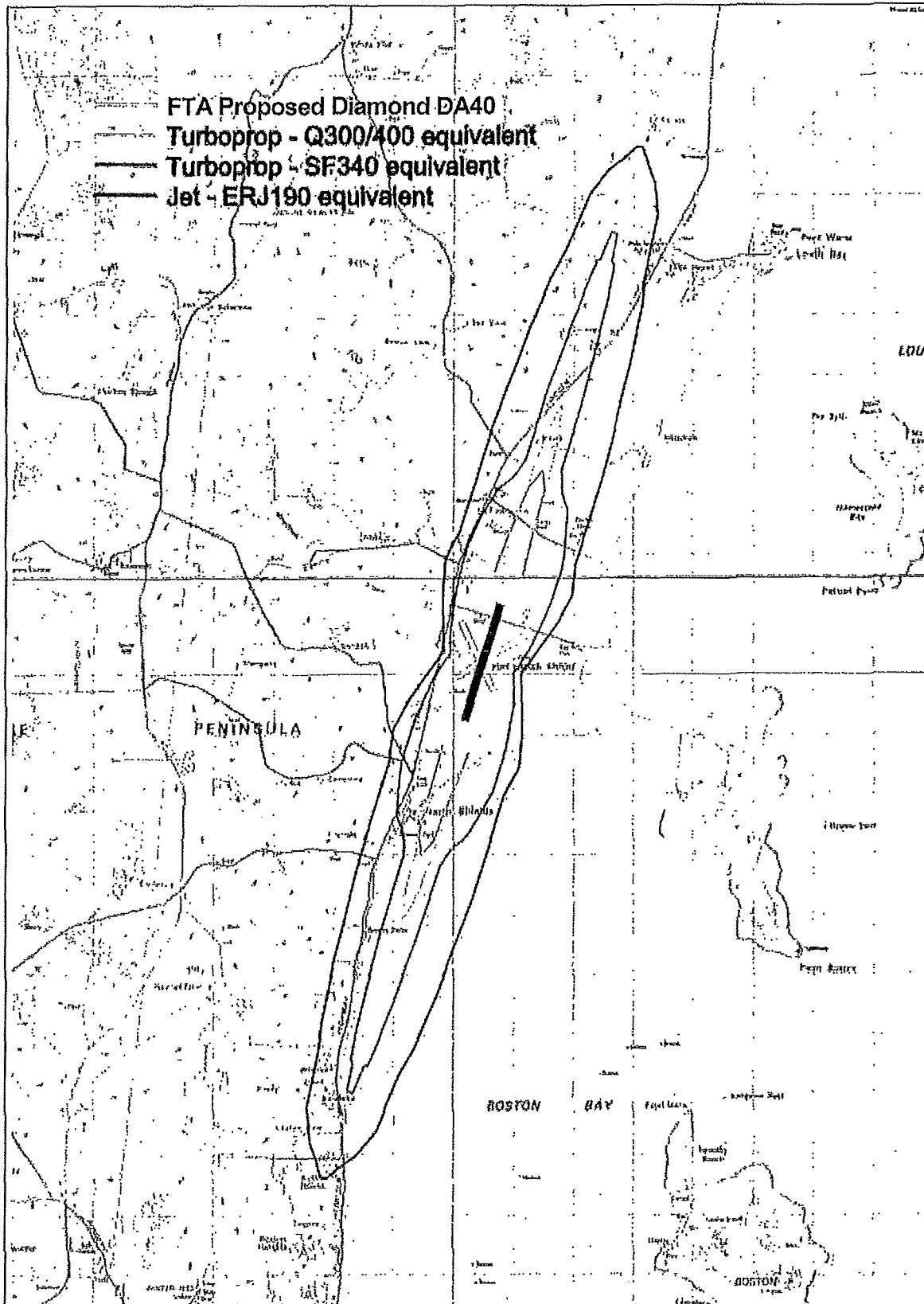
Noise data in the form of 70 dB(A) contours for each group of aircraft was plotted from AS2021 which provides noise levels at various distances and offsets from the runway ends based on actual measurements recorded at Sydney Airport. The contours show:

- *Noise intrusion by the 70 dB(A) noise level into residential area in North Shields from current Saab 340 operations will be reduced significantly when the existing fleet is replaced with later model turbo prop aircraft equivalent to Q400 aircraft.*
- *Introduction of jet aircraft will generate single event noise levels above current levels;*
- *Noise intrusion in the form of 70 dB(A) contours into the residential area will not be increased by the DA40 in comparison to the existing fleet or potential future jet aircraft.*

APPENDIX A NOISE CONTOURS FROM THE MASTERPLAN



APPENDIX B PROPOSED AIRCRAFT CONTOUR





REAL PROPERTY ACT, 1988



South Australia

The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Certificate of Title - Volume 5708 Folio 74

Parent Title(s)	CT 5345/407			
Creating Dealing(s)	AQ 8577149A, RTD 8720703			
Title Issued	08/11/1999	Edition	1	Edition Issued 08/11/1999

Estate Type

FEE SIMPLE

Registered Proprietor

DISTRICT COUNCIL OF LOWER EYRE PENINSULA
OF PO BOX 41 CUMMINS SA 5631

Description of Land

ALLOTMENT 9 DEPOSITED PLAN 52836
IN THE AREA NAMED NORTH SHIELDS
HUNDRED OF LOUTH

Easements

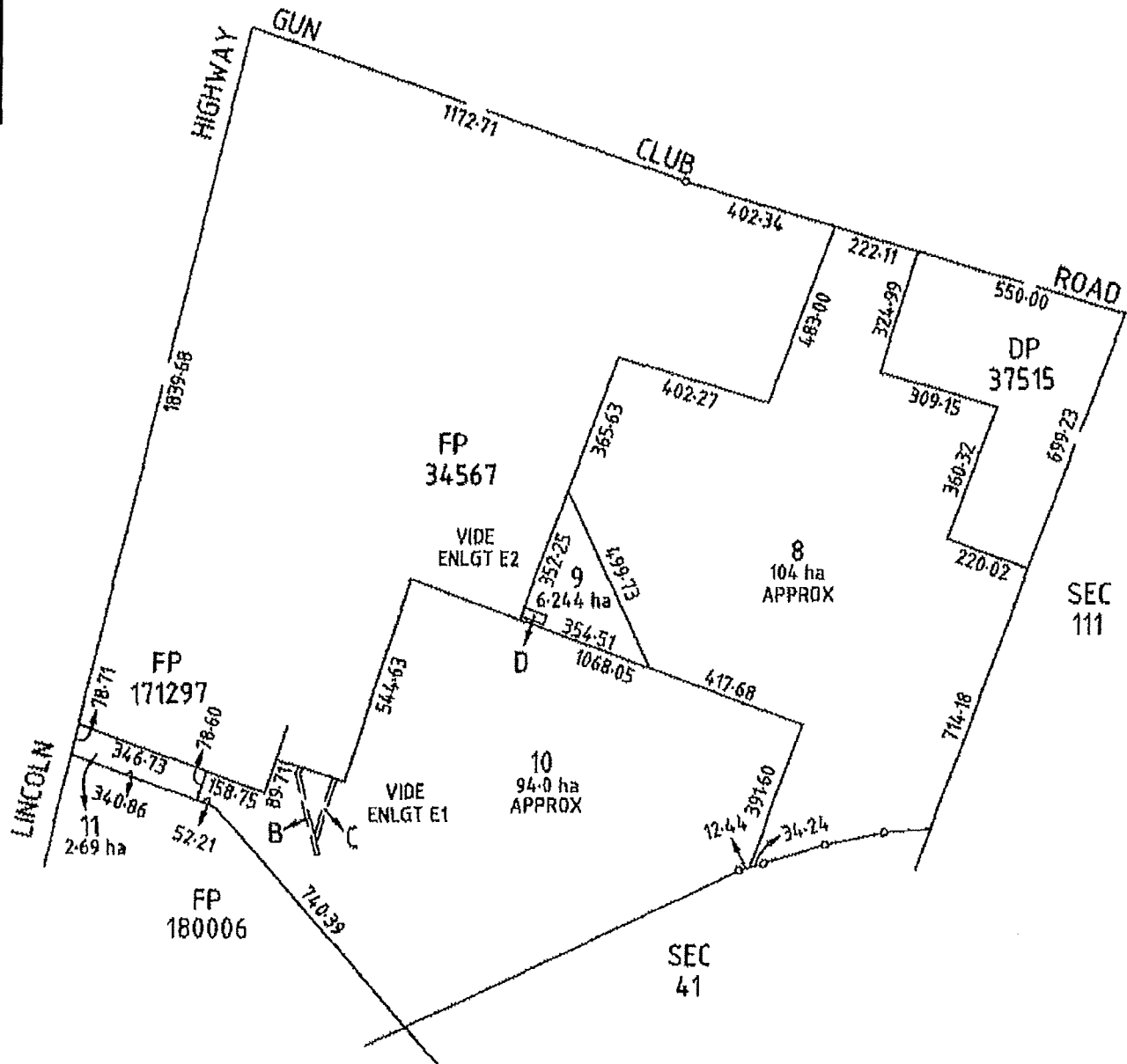
SUBJECT TO EASEMENT(S) OVER THE LAND MARKED D TO THE COMMONWEALTH OF AUSTRALIA (T 3337573)

Schedule of Dealings

NIL

Notations

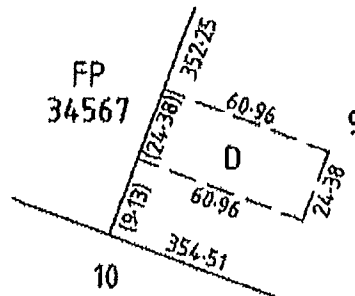
Dealings Affecting Title	NIL
Priority Notices	NIL
Notations on Plan	NIL
Registrar-General's Notes	NIL
Administrative Interests	NIL



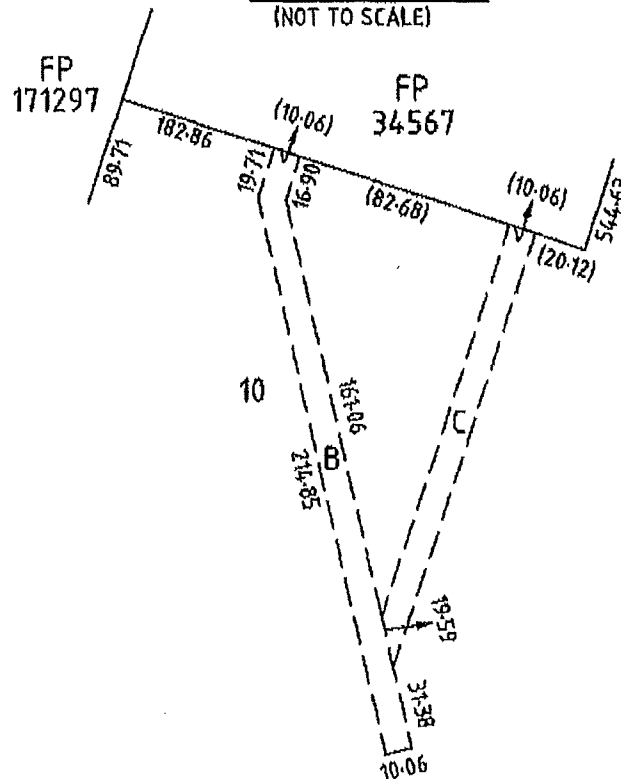
0 150 300 450 600 Metres



ENLARGEMENT E2
(NOT TO SCALE)



ENLARGEMENT E1
(NOT TO SCALE)





REAL PROPERTY ACT, 1933



South Australia

The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Certificate of Title - Volume 5708 Folio 75

Parent Title(s)	CT 5345/408		
Creating Dealing(s)	AQ 8577148A, RTD 8720703		
Title Issued	08/11/1999	Edition 1	Edition Issued 08/11/1999

Estate Type

FEE SIMPLE

Registered Proprietor

DISTRICT COUNCIL OF LOWER EYRE PENINSULA
OF PO BOX 41 CUMMINS SA 5631

Description of Land

ALLOTMENT 10 DEPOSITED PLAN 52836
IN THE AREA NAMED NORTH SHIELDS
HUNDRED OF LOUTH

Easements

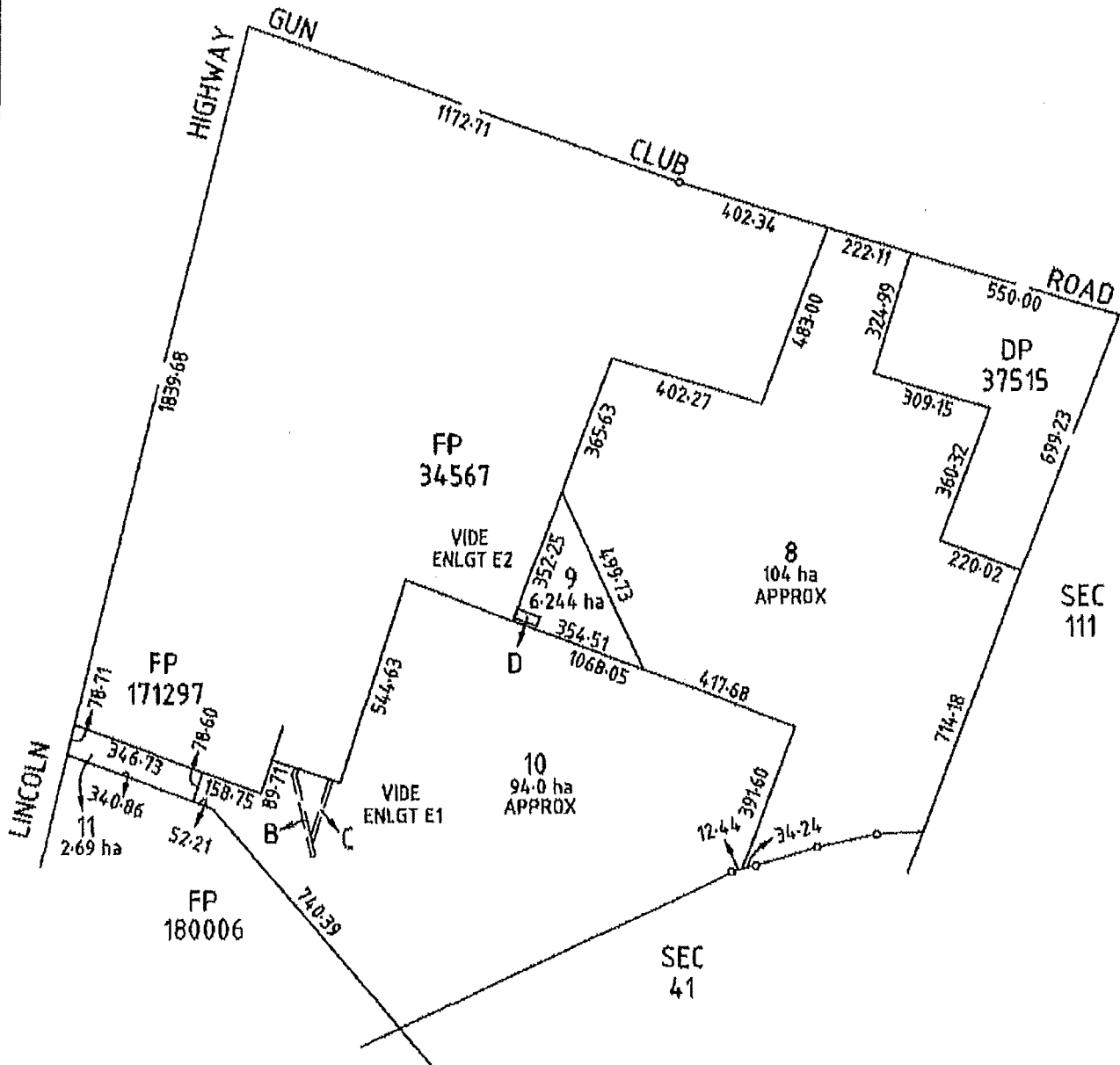
SUBJECT TO EASEMENT(S) OVER THE LAND MARKED B AND C TO THE COMMONWEALTH OF AUSTRALIA (T 1680311 AND T 2916638 RESPECTIVELY)

Schedule of Dealings

NIL

Notations

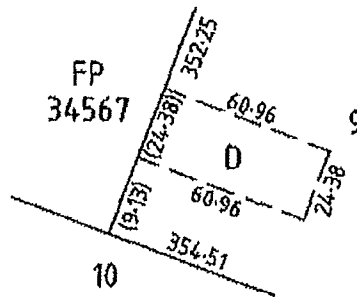
Dealings Affecting Title	NIL
Priority Notices	NIL
Notations on Plan	NIL
Registrar-General's Notes	NIL
Administrative Interests	NIL



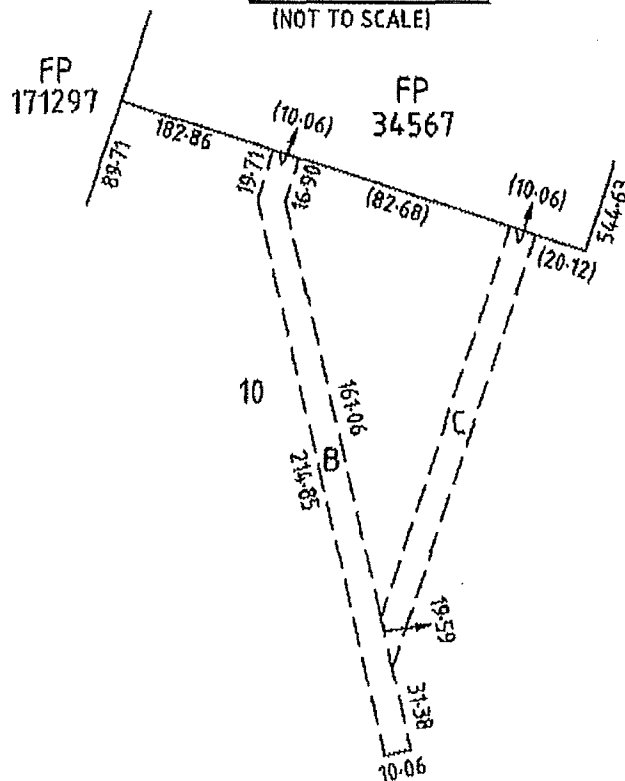
0 150 300 450 600 Metres



ENLARGEMENT E2
(NOT TO SCALE)



ENLARGEMENT E1
(NOT TO SCALE)





REAL PROPERTY ACT, 1988



South Australia

The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Certificate of Title - Volume 6111 Folio 59

Parent Title(s)	CT 5346/3, CT 5511/816, CT 5804/39				
Creating Dealing(s)	RTC 11917604				
Title Issued	21/05/2013	Edition	1	Edition Issued	21/05/2013

Estate Type

FEE SIMPLE

Registered Proprietor

DISTRICT COUNCIL OF LOWER EYRE PENINSULA
OF PO BOX 41 CUMMINS SA 5631

Description of Land

ALLOTMENT COMPRISING PIECES 21, 22 AND 23 DEPOSITED PLAN 86780
IN THE AREA NAMED NORTH SHIELDS
HUNDRED OF LOUTH

Easements

SUBJECT TO EASEMENT(S) OVER THE LAND MARKED A TO DISTRIBUTION LESSOR CORPORATION (SUBJECT TO LEASE 8890000) (TG 7770189)

TOGETHER WITH EASEMENT(S) OVER THE LAND MARKED B APPURTENANT ONLY TO THE LAND MARKED X (T 1680311)

TOGETHER WITH EASEMENT(S) OVER THE LAND MARKED C AND D APPURTENANT ONLY TO THE LAND MARKED X (T 2916638)

TOGETHER WITH EASEMENT(S) OVER THE LAND MARKED C AND D APPURTENANT ONLY TO THE LAND MARKED Y (T 3337573)

Schedule of Dealings

Dealing Number	Description
8199693	LEASE TO AIRSERVICES AUSTRALIA COMMENCING ON 1/7/1994 AND EXPIRING ON 30/6/2044 OF PORTION (A IN GP 294/1992)
8863155	ENCUMBRANCE TO COMMONWEALTH OF AUSTRALIA

Notations

Dealings Affecting Title	NIL
Priority Notices	NIL
Notations on Plan	NIL

Registrar-General's Notes

PLAN FOR LEASE PURPOSES VIDE G294/1992
AMENDMENT TO DIAGRAM VIDE 24/2014



Administrative Interests

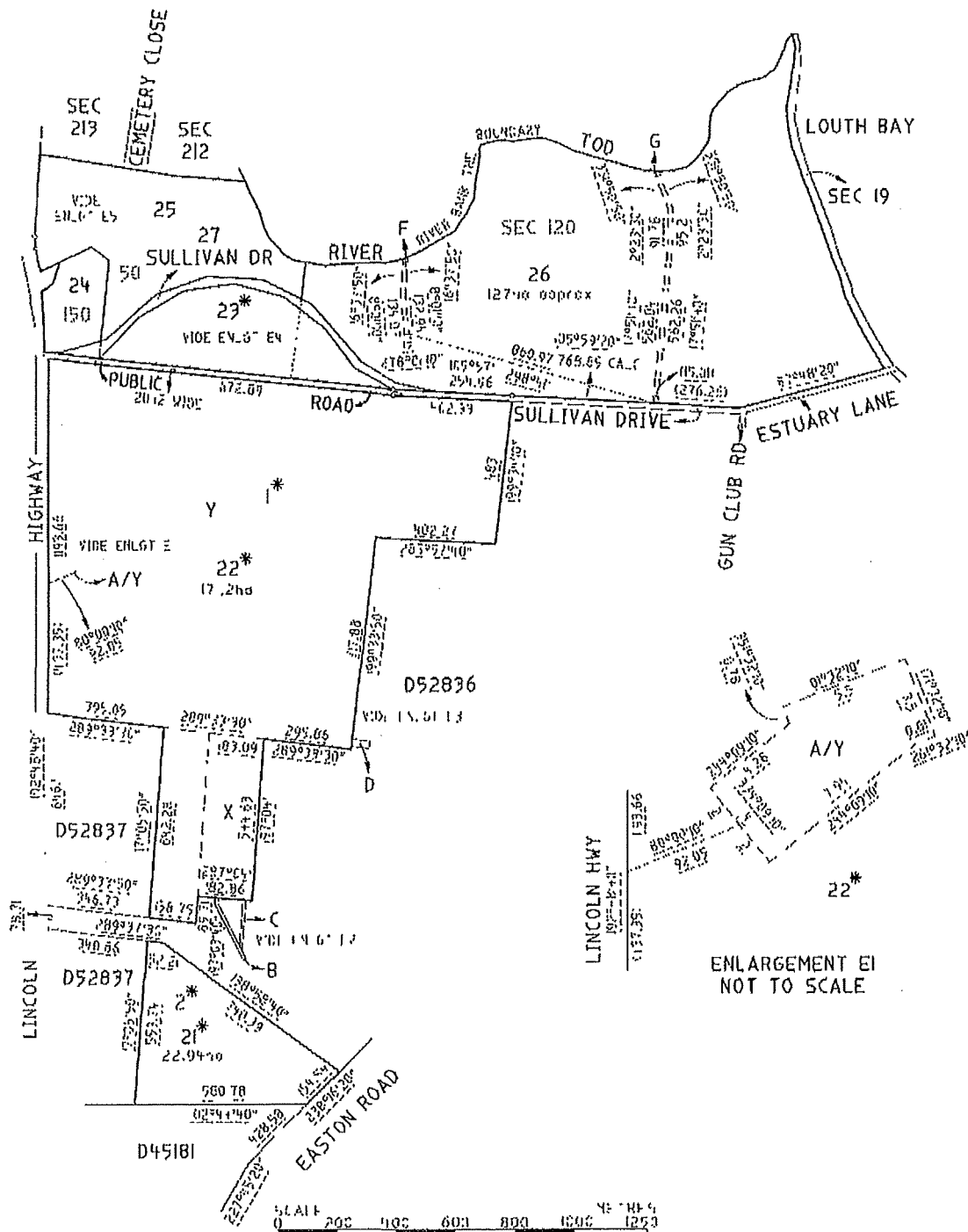
NIL



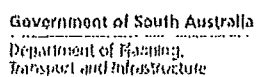
PIECES SCHEDULE

ONE ALLOTMENT COMPOSITE	TOTAL AREA
2*, 22* & 23*	212.4ha

* Asterisk denotes P/L/L Identifier only.



NATURAL BOUNDARY AND SURVEY INFORMATION IS DERIVED FROM DEPOSITED PLAN 86780



Product	Register Search (CT 6111/59)
Date/Time	14/01/2019 10:19AM
Customer Reference	51142
Order ID	20190114002606
Cost	\$28.75

** Asterisk denotes PIECE Identifier only

